

Title	Planning Applications
To:	Planning Control Committee
On:	24 January 2017
By:	Development Manager
Status:	For Publication

Executive Summary

The attached reports present members with a description of various planning applications, the results of consultations, relevant policies, site history and issues involved.

My recommendations in each case are given in the attached reports.

This report has the following implications

Township Forum/ Ward: Identified in each case.

Policy: Identified in each case.

Resources: Not generally applicable.

Equality Act 2010: All planning applications are considered in light of the Equality Act 2010 and associated Public Sector Equality Duty, where the Council is required to have due regard for:
The elimination of discrimination, harassment and victimisation;
The advancement of equality of opportunity between persons who share a relevant protected characteristic and person who do not share it;
The fostering of good relations between persons who share a relevant protected characteristic and person who do not share it; which applies to people from the protected equality groups.

Human Rights: All planning applications are considered against the provisions of the Human Rights Act 1998.

Under Article 6 the applicants (and those third parties who have made representations) have the right to a fair hearing and to this end full consideration will be given to their comments.

Article 8 and Protocol 1 of the First Article confer a right to respect private and family life and a right to the protection of property, ie peaceful enjoyment of one's possessions which could include a person's home, and other land and business assets.

In taking account of the Council policy as set out in the Bury Unitary Development Plan 1997 and all material planning considerations, I have concluded on balance that the rights conferred upon the applicant/ objectors/ residents/ other interested party by Article 8 and Article 1 of the First Protocol may be interfered with, since such interference is in accordance with the law and is justified in the public interest. Any restriction of these rights posed by refusal/ approval of the application is legitimate since it is proportionate to the wider benefits of such a decision, is based

upon the merits of the proposal, and falls within the margin of discretion afforded to the Council under the Town & Country Planning Acts.

The Crime and Disorder Act 1998 imposes (without prejudice to any other obligation imposed on it) a duty upon the Council to exercise its functions and have due regard to the likely effect of the exercise of its functions on, and the need to do all that it reasonably can to prevent crime and disorder in its area. In so doing and on making planning decisions under the Town and Country Planning Acts, the Planning Control Committee shall have due regard to the provisions of the Crime and Disorder Act 1998 and its implications in the exercise of its functions.

Development Manager

Background Documents

1. The planning application forms and plans submitted therewith.
2. Certificates relating to the ownership.
3. Letters and Documents from objectors or other interested parties.
4. Responses from Consultees.

FOR FURTHER INFORMATION ON THE CONTENTS OF EACH REPORT PLEASE CONTACT INDIVIDUAL CASE OFFICERS IDENTIFIED IN EACH CASE.

01	Township Forum - Ward:	Prestwich - Sedgley	App No.	60045
	Location:	Texaco Petrol Station, Bury Old Road, Prestwich, Manchester, M25 0EY		
	Proposal:	Demolition of existing structures and erection of 2 retail (A1) units and synagogue with associated car parking, landscaping and access arrangement		
	Recommendation:	Refuse	Site Visit:	Y
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02	Township Forum - Ward:	Whitefield + Unsworth - Unsworth	App No.	60527
	Location:	Unit 5, Croft Lane Industrial Estate, Croft Lane, Bury, BL9 8QG		
	Proposal:	Change of use from light industrial (Class B1) and general industrial (Class B2) to personal training studio (Class D2)		
	Recommendation:	Approve with Conditions	Site Visit:	N
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03	Township Forum - Ward:	Prestwich - Holyrood	App No.	60567
	Location:	Prestwich Heys A.F.C, Adie Moran Park, Sandgate Road, Whitefield, Manchester, M45 6WG		
	Proposal:	Variation of conditions 4, 6 & 11 of planning permission 59412		
	Recommendation:	Approve with Conditions	Site Visit:	N
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04	Township Forum - Ward:	Bury West - Church	App No.	60688
	Location:	29 Bolton Road, Bury, BL8 2AB		
	Proposal:	Change of use from A1 (retail) to A5 (hot food takeaway) with addition of flue to rear elevation		
	Recommendation:	Approve with Conditions	Site Visit:	N
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05	Township Forum - Ward:	Ramsbottom and Tottington - Ramsbottom	App No.	60708
	Location:	Former Ramsbottom Police Station, Bridge Street, Ramsbottom, Bury, BL0 9AB		
	Proposal:	Demolition of former police station and erection of building to include 2 no. shop units with 8 no. apartments above		
	Recommendation:	Approve with Conditions	Site Visit:	Y
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06	Township Forum - Ward: Bury East - Redvales	App No. 60732
	Location: Hatfields, Manchester Road, Bury, BL9 9XX	
	Proposal: Demolition of showroom section of existing facility and erection of new showroom, refurbishment and extension of workshop to include new MOT and valet bays and amendments to site parking and display layouts.	
	Recommendation: Approve with Conditions	Site Visit: N
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07	Township Forum - Ward: Radcliffe - North	App No. 60784
	Location: 41 Bury Old Road, Ainsworth, Bolton, BL2 5PF	
	Proposal: Change of use of stable to dwelling with single storey extension	
	Recommendation: Approve with Conditions	Site Visit: N
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08	Township Forum - Ward: Radcliffe - West	App No. 60810
	Location: Land at Bolton Street/Holly Bank Street, Radcliffe, Manchester, M26 3SS	
	Proposal: Erection of 9 no. garages	
	Recommendation: Approve with Conditions	Site Visit: N
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09	Township Forum - Ward: Prestwich - St Mary's	App No. 60836
	Location: 7 Fairfax Road, Prestwich, Manchester, M25 1AS	
	Proposal: Change of use from Citizens Advice Bureau (Class A2) to wine bar (Class A4) and alterations to front/rear elevations	
	Recommendation: Approve with Conditions	Site Visit: N
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10	Township Forum - Ward: Ramsbottom and Tottington - Ramsbottom	App No. 60878
	Location: Land adjacent to 12 Est Bank Road, Ramsbottom, Bury, BL0 9RA	
	Proposal: Change of use of land for the extension of residential curtilage and erection of boundary fencing	
	Recommendation: Approve with Conditions	Site Visit: N
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11	Township Forum - Ward: Ramsbottom and Tottington - Ramsbottom	App No. 60879
	Location: Land adjacent to 361 Bolton Road West, Ramsbottom, Bury, BL0 9QZ	
	Proposal: Change of use of land for the extension of residential curtilage and erection of boundary fencing	

Recommendation: Approve with Conditions

Site Visit: N

12 Township Forum - Ward: Prestwich - Holyrood **App No.** 60929

Location: 478 Bury Old Road, Prestwich, Manchester, M25 1NL

Proposal: Alterations to shop front, conversion from 1 no. unit to 2 no. units and change of use from retail (Class A1) to retail and hot food takeaway (Class A1 & A5) with flue at rear

Recommendation: Approve with Conditions

Site Visit: N

13 Township Forum - Ward: Bury East **App No.** 60939

Location: Land adjacent to 32 Nuttall Street, Bury, BL9 7EW

Proposal: Erection of 1 no. attached dwelling

Recommendation: Approve with Conditions

Site Visit: N

14 Township Forum - Ward: Bury East **App No.** 60957

Location: Fairfield Primary School, Rochdale Old Road, Bury, BL9 7SD

Proposal: Classroom extension and covered corridor; Replacement parking spaces at front

Recommendation: Approve with Conditions

Site Visit: N

Ward: Prestwich - Sedgley

Item 01

Applicant: Unimore Limited

Location: Texaco Petrol Station, Bury Old Road, Prestwich, Manchester, M25 0EY

Proposal: Demolition of existing structures and erection of 2 retail (A1) units and synagogue with associated car parking, landscaping and access arrangement

Application Ref: 60045/Full

Target Date: 02/09/2016

Recommendation: Refuse

This application is being presented to Planning Control Committee as Councillor Caserta has called the application in. A refusal would ordinarily be delegated to officers.

Description

The site currently contains a petrol filling station, associated shop and car wash. The site is located centrally within a local shopping centre with access from Bury Old Road. The petrol pumps are located near to Bury Old Road with the shop and car wash located to the rear.

There are commercial buildings to the north and south, which front onto Bury Old Road. There are residential properties to the north (Albert Road), east and west with allotments beyond.

Planning permission was granted in October 2013 for the demolition/removal of the shop, car wash and petrol pumps and the erection of a single storey building containing an Class A1 retail unit with an ancillary Class A3 cafe unit (combined floor space of 418 square metres). This consent has not been implemented.

The proposed development seeks permission for the demolition/removal of the existing shop, car wash and petrol pumps and the erection of a two storey building containing retail (Class A1) units at ground floor and a synagogue at first floor. A new access would be formed onto Bury Old Road, which would connect to the car park, with 15 spaces. The proposed opening hours for the proposed retail units would be from 08.00 to 23.00 on Sunday to Friday and closed on Saturdays. The opening hours for the proposed synagogue would be from 07.00 to 08.00 on Monday to Friday, 08.00 to 21.00 on Saturdays and from 08.00 to 09.00 on Sunday. This congregation currently operate at Park View Road and are seeking an alternative venue/site.

Relevant Planning History

33851 - Redevelopment of existing petrol filling station at 19 Bury Old Road, Prestwich. Approved with conditions - 19 March 1998

42819 - Prior approval determination - 14.7m high streetworks pole style telecommunications structure accommodating 3 no./ shrouded antennae and 2 equipment cabins at site outside 19 Bury Old Road, Prestwich. Refused - 13 July 2004.

53747 - Prior notification for 14.8 metre high streetworks pole including 3 antennae and equipment cabinets at Prestwich Service Station, 19 Bury Old Road, Prestwich. Prior approval required and granted - 19 April 2011.

56277 - Demolition of existing structures and erection of A1 retail unit with ancillary A3 use and associated car parking, landscaping and access arrangement at Texaco Petrol Station, Bury Old Road, Prestwich. Withdrawn - 15 July 2013.

This application was withdrawn as there were issues relating to the access and servicing

arrangements.

56526 - Demolition of existing structures and erection of an A1 Use Class retail unit, with ancillary A3 use and associated car parking, landscaping and access arrangement (resubmission of 56277) at Texaco petrol station, Bury Old Road, Prestwich. Approved with conditions - 10 October 2013.

Publicity

The neighbouring properties were notified by means of a letter on 8 June and a press notice was published in the Bury Times on 16 June 2016. Site notices were posted on 15 June 2016.

A letter of support has been received from Councillor Quinn, which has raised the following issues:

- Support the application as I know the congregation well and am a governor of nearby Park View School.
- They are a small congregation and have been excellent and considerate neighbours to the school.
- In 20 years I have never seen them use cars to attend the synagogue.
- Wish them every success with their application.

112 letters of support have been received from the occupiers of the following properties:

PRESTWICH - 1, 8, 18, 19 Ravensway, 20 George Street, 19 Castle Hill Road, 47, 76, 76A, 105, 122 Kings Road, 9 Ravens Close, Park Hill (5), 8, 12, 30 Bury Old Road, 12 Dellcot Close, 5, 12, 16 Salisbury Drive, 90 Glebelands Road, 1 Lancaster Drive, 3, 5, 17 Hereford Drive, 27 Hilton Crescent, 66A, 80, 118, 180 Albert Avenue, 17 Woodland Crescent, 38 Bishops Road, 7, 17 Lichfield Drive, 4 Lancaster Drive, 5, 6, 45 Tewkesbury Drive, 2B Mather Avenue, 4 Parksway, 3, 11, 15 Sedgley Park Road, 14 Canterbury Drive, 16 Breeze Mount, 7, 66 Richmond Avenue, 14, 16 East Meade, 14 Silverdale Avenue, 15 Sedgley Avenue, 8 Barnhill Avenue, 8, 15, 38, 68, 82 Park Road, 3 Oakfield, 2 Links Crescent, 7 Harrogate Avenue.

WHITEFIELD - 37 Standmoor Road.

SALFORD - 11 Vernon Road, 1, 11 Park Lane, 19 Old Hall Lane, 55 Broom Lane, 20 The Mount, 8, 22, 27, 38, 77, 88, 97 Cavendish Road, Flat 2, Grosvenor Court, 11 New Hall Avenue, 13 Sommerville Court, Flat 4, Hazel Slack, Limefield Court, 55, 58 Upper Park Road, 18 Eccleston Place, 397 Great Cheetham Street East, 34 Stanley Road, 10 Eskrigge Close, 18 Rowan Way, Flat 7, Ingledene Court, Manchester & District Council of Synagogues, Holden Road, 2A Worthington Drive, 468B Bury New Road, 13 The Vineyard, Flat 2, 4, Bedford Court,

MANCHESTER - 23, 38, 54 Park Road, New Hall Road, 6, 8 Kersal Crag, 2 Deanery Gardens, 57 - 59, 95 Leicester Road, 75, 126 Neville Road, 8 Waterpark Road, Manchester Beth Din, Bury Old Road, 97 Singleton Road,

CRUMPSALL - 8 Melton Road,

GATESHEAD - 178 Prince Consort Road,

The letters of support have raised the following issues:

- The relocation would provide a fantastic springboard for all kinds of communal activities for the benefit of local residents - youth groups, prayer services, public lectures, charity drives, academic research facilities and hosting of community support services.
- Believe this building will serve a tremendous need.
- A convenience supermarket would be of benefit to the wider public.
- Objections based upon traffic are clearly unfounded as the area would not be any busier than current usage.
- Have been a member of this synagogue for 40 years but can no longer attend as it is too far for me to walk. The proposed site would be within walking distance of my current home.
- Been a member for 15 years and attended for over 30 years. Over this time the synagogue has lost members as it is on the periphery of the community.
- As it is a small building, there are no adequate facilities for young children, which means

we lose young married.

- Park View Road is becoming increasingly congested making walking there on the Sabbath and other Holy Days is becoming difficult.
- We have received a lot of positive comments from potential members.
- This ideal location will enable elderly people and kids who can't walk far to attend services and social programs arranged by the synagogue.
- We need a local synagogue within a convenient walking distance as local demographic changes have resulted in a lack of such facilities.
- The Jewish community lives much closer to the proposed site and it will enhance the continuity of the synagogue if permission is granted.
- There would be no congestion at the site on the sabbath.
- As the shop and Synagogue will be open less hours than the existing filling station there would be less stress on the local roads than there is now.
- There is a need for a local kosher store, which would be of use for those without vehicles.
- The proposal would provide a new centre for our youth in an age where facilities for them are in decline.
- The local area has a dramatically expanding Jewish Orthodox population, which needs to be catered for. The proposed development would help to service the needs of the Jewish community.
- The Jewish community is growing quickly and there is a desperate need for more synagogues and kosher supermarkets. We are grateful for your ongoing support to ensure that the community can grow.
- Moreover, to relocate the synagogue from its current location will undoubtedly encourage more to join.
- The proposed development would increase footfall in the area.
- The proposed development would improve the existing derelict site.

6 letters have been received from the occupiers of 137, 180 Albert Avenue, 6 St Mary's Road, 6 Park Hill Bury Old Road, 1 Ravensway, which have raised the following issues:

- Concerned about parking, which is a real difficulty at the moment.
- Many people from that block who work/shop there park on Albert Avenue, which is inconvenient for residents.
- I would be ok with the plans if the Council made Albert Avenue a residents only parking space.
- Overall massing/scale of development is out of character with the adjacent buildings.
- Privacy distances between proposed building and existing dwellings seem inadequate.
- Parking facilities are inadequate and would add to existing congestion.
- Proposed building would be 10.8 metres as my garden is 1.8 metres lower than the site.
- Concerns relating to operating hours, light pollution and noise pollution.
- The synagogue does not need to move. It is fine where it is situated now - waste of money.

The objectors and supporters have been notified of the Planning Control Committee meeting.

Consultations

Traffic Section - Recommend refusal - see issues section.

Drainage Section - Comments awaited.

Environmental Health - Contaminated Land - No objections, subject to the inclusion of conditions relating to contaminated land.

Environmental Health - Commercial Section - Comments awaited.

Environmental Health - Pollution Control - Comments awaited.

Waste Management - Comments awaited.

Environment Agency - No objections, subject to the inclusion of conditions relating to surface water drainage and contaminated land.

Design for security - No objections.

United Utilities - No objections, subject to the inclusion of conditions relating to foul and

surface water drainage.

GM Fire Service - Comments awaited.

GM Ecology Unit - No objections, subject to the inclusion of informatives relating to bats and nesting birds.

National Grid - Comments awaited.

Unitary Development Plan and Policies

EN1/1	Visual Amenity
EN1/2	Townscape and Built Design
EN1/3	Landscaping Provision
EN1/4	Street Furniture
EN1/5	Crime Prevention
EN1/7	Throughroutes and Gateways
EN1/8	Shop Fronts
EN6	Conservation of the Natural Environment
EN6/3	Features of Ecological Value
EN7	Pollution Control
EN7/2	Noise Pollution
EN7/5	Waste Water Management
S1/4	Local Shopping Centres
S2/1	All New Retail Proposals: Assessment Criteria
S4/4	Car Showrooms, Car Sales Areas and Petrol Filling Stns
HT2/4	Car Parking and New Development
HT4	New Development
HT5/1	Access For Those with Special Needs
CF1/1	Location of New Community Facilities
SPD6	Supplementary Planning Document 6: Alterations & Extensions
SPD11	Parking Standards in Bury
NPPF	National Planning Policy Framework

Issues and Analysis

The following report includes analysis of the merits of the application against the relevant policies of both the National Planning Policy Framework (NPPF) and the adopted Bury Unitary Development Plan (UDP) together with other relevant material planning considerations. The policies of the UDP that have been used to assess this application are considered to be in accordance with the NPPF and as such are material planning considerations. For simplicity, just the UDP Policy will be referred to in the report, unless there is a particular matter to highlight arising from the NPPF where it would otherwise be specifically mentioned.

Principle (retail) - Policy S1/4 states that the Council will seek to maintain and enhance local shopping centres and will encourage the provision of a range of shopping facilities required to serve purely local needs.

Policy S2/1, which states that the Council will support new retail development which accord with the following:

- are within or immediately adjoining the main shopping area of existing centres;
- sustain or enhance the vitality or viability of a centre;
- are accessible by and would encourage greater use by public transport;
- are in conformity with other policies and proposals of the Plan

In addition, where a proposal would lead to more than 40% of the frontage being in non-retail use, the Council will take into account the following factors:

- the location and prominence of the proposal within the frontage;
- the number, distribution and proximity of other premises in non-retail use;
- the particular nature and character of the use proposed, including the level of activity associated with it.

The proposed development involves the provision of 2 Class A1 retail units. The application site lies within the local shopping centre and would have a combined floorspace of 483

square metres. The proposed retail units would act as an anchor for the centre and provide linked trips, which would generate trade for existing businesses. As such, the proposed development would have a positive impact upon the vitality and viability of the centre. Therefore, the proposed development would be in accordance with Policies S1/4 and S2/1 of the Bury Unitary Development Plan.

Principle (Community use) - Policy CF1/1 states that proposals for new and improved community facilities will be considered with regard to the impact upon residential amenity and the local environment; traffic generation and car parking; the scale and size of the development; accessibility by public and private transport; the needs and requirements of the disabled.

The proposed development would provide a synagogue at first floor level. The site would be accessible by public and private transport and would be located in the local area, it would be intended to serve. The issues of residential amenity, traffic generation, the scale and size of the development and the needs and requirements of the disabled will be discussed below. Therefore, the proposed development would be acceptable in principle and would be in accordance with Policies CF1/1 of the Bury Unitary Development Plan.

Design and layout - The proposed building accommodating two uses, would be two storeys in height and would be located towards the rear of the site. The proposed building would be 9 metres in height and would be some 1.4 metres higher than the existing buildings. However, the existing buildings would be located towards the front of the site and given the distance between them, the difference in height would not be perceptible.

The proposed development would incorporate a modern design with floor to ceiling glazing for the retail units on the front elevation as a focal point. The proposed building would be constructed from timber cladding, grey brickwork and render, which would be acceptable and would add visual interest to the elevations. Therefore, the proposed development would not be a prominent feature in the streetscene and would be in accordance with Policies EN1/2 and S2/1 of the Bury Unitary Development Plan.

Impact upon surrounding area - The proposed site plan indicates that the trees on the perimeter of the site would be retained and a condition relating to tree protection measures would be added to secure this. The proposed bin store would be located in the western corner of the site and would be accessed from the loading bay in the car park. A timber fence would be erected to the boundaries of the site, which would not be prominent in the locality. Therefore, the proposed development would be in accordance with Policies EN1/2 and S2/1 of the Bury Unitary Development Plan.

Impact upon residential amenity - SPD6 provides guidance on aspect standards and as such, is a useful guide in this instance. The aspect standards states that there should be a minimum of 20 metres between directly facing habitable windows and 13 metres between a habitable room window and a two storey blank wall. If there is an additional storey in height, a further 3 metres should be added to the aspect standard.

The proposed building would be 9 metres in height, which would be the equivalent of a three storey building. There would be between 16.75 metres and 17.65 metres between the residential properties on Albert Avenue and the gable elevation of the proposed building. Whilst there are 6 windows within the gable elevation of the proposed building, these would be obscure glazed and non-openable. Therefore, the proposed development would be in excess of the 16 metre aspect standard and would not have an adverse impact upon the amenity of the neighbouring properties. Although the massing of the building would be somewhat different to that of a house.

Noise - There is a petrol station and hand car wash on site currently and there are no restrictions in terms of hours of opening. The proposed development would involve the provision of retail units, which would be open from 08.00 to 22.00 on Mondays to Fridays and Sundays. The applicant is not applying to be open on Saturdays for religious reasons.

The main sources of noise would be from cars and deliveries to the site and the proposed levels of noise would be significantly lower than the existing uses at the site. It is proposed to restrict the hours of delivery to 08.00 to 22.00, which would reduce noise levels further. Therefore, it is considered that subject to conditional control, the proposed development would not have an significantly adverse impact upon the neighbouring properties. Therefore, the proposed development would be in accordance with Policy EN7/2 of the Bury Unitary Development Plan.

Ecology/bats - A bat survey was submitted as part of the application and concludes that the building is completely free of bat roosting potential. It continues that even if demolition is delayed it is unlikely that this building will develop significant roosting potential within the next few years. GM Ecology Unit is satisfied with the conclusions of the report and has no objections to the proposed development, subject to the inclusion of informatives in relation to bats and nesting birds. Therefore, the proposed development would not cause harm to a protected species and would be in accordance with Policy EN6/4 of the Bury Unitary Development Plan and the NPPF.

Highways issues - The northern access to the site would be modified to act as the sole access into the application site. The southern access would be retained as the access to the rear of the adjacent terrace of shops. The Traffic Section has assessed the proposed access and has no objections to the access, but is recommending refusal for other reasons (see below). Therefore, this aspect of the proposed development would not be detrimental to highway safety and would be in accordance with Policy S2/1 of the Bury Unitary Development Plan.

Parking - SPD11 states that the maximum parking standards for a retail unit is 1 space per 30 square metres and for a place of worship is 1 space per 5 square metres of public floor area. This equates to 16 spaces for the retail units and 68 spaces for the synagogue (considering the public areas involved only), making a maximum of 83 spaces.

The proposed development would provide 15 parking spaces including 2 disabled parking bays. The proposed parking provision of 15 spaces would be well below the maximum level for the combined development.

The agent states within the Planning Statement that the application site is located in 'A densely populated area that is popular with people of the Jewish faith and it is foreseen that the synagogue will be extremely popular with the existing community and with other neighbours who are not currently members.' A supporter of the scheme is hopeful that the relocation would provide a fantastic springboard for all kinds of communal activities for the benefit of local residents - youth groups, prayer services, public lectures, charity drives, academic research facilities and hosting of community support services. The proposed synagogue would be larger than the current synagogue and a larger floor area would increase demand.

In addition, the transport statement confirms that, 'The parking provision is for the retail units only, as members of the Synagogue will walk to the site on the Sabbath as car use is not permitted for religious reasons. Furthermore the synagogue will be used during the week for prayer services.'

Whilst the fact that the congregation would not drive to the site on the Sabbath is acknowledged, there would be a requirement for parking for services/lectures/other events that are held on days other than the sabbath. The two uses (retail and synagogue) would overlap in terms of usage and timing and it would not be possible to share the parking provision proposed without creating parking demand problems. In addition, the site is located on Bury Old Road, which is part of the key route network and has double yellow lines immediately in the vicinity of the site. As such, it is not possible to park on-street, which would lead to parking being sought on the residential streets nearby.

Key issues of concern relating to parking derive from the dual and overlapping uses and the

fact that the applicant makes it clear that this synagogue would be used more frequently other than for a single/limited prayer gatherings. The additional uses proposed may include youth groups, prayer services, public lectures, charity drives, academic research facilities and hosting of community support services. The planning regime can neither restrict the use nor can it restrict the number of members. The scale of the facility (floorspace/rooms/uses) proposed would be such that it would be of much wider interest than to those necessarily within close walking distance and popularity could easily be driven by the scale of the development and services offered. Were the building much smaller, this would be self regulating. As such, it is not unreasonable to consider the obverse as reasonable.

The likely interest of the use and floorspace combined with the daily demands offered by the ground floor retail use would generate significant demands in servicing and traffic, which parking provisions cannot be reasonably satisfied on this site.

It was suggested that parking on an alternative, but nearby site should be provided, which could be used at busy periods. The agent has investigated this, but no suitable sites in terms of size or location are available.

Therefore, the proposed development would not provide an adequate level of car parking provision, which would lead to parking taking place on the adjacent streets. This in turn would have an adverse impact upon the safe and efficient operation of the highway network. Therefore, the proposed development would be in conflict with Policies EN1/2, EN1/7, HT2/4, HT4 and CF1/1 of the Bury Unitary Development Plan and SPD11.

Access - Level access would be provided to the building and two disabled parking bays would be located in close proximity to the entrance. A lift would be provided within the building and a disabled toilet would be provided at first floor level. Therefore, the proposed development would be accessible for all and would be in accordance with Policies HT5/1 and CF1/1 of the Bury Unitary Development Plan.

Planning obligations - The previous application for retail unit was accompanied by a unilateral undertaking (legal agreement), which stated that the retail units would only be serviced by a transit van sized vehicle as this was the largest vehicle that could operate within the turning head.

The applicant has indicated that they would be happy to submit the unilateral undertaking for this application. However, due to the concerns relating to parking provision, this has not been progressed on this application.

Statement in accordance with Article 35(2) Town and Country Planning (Development Management Procedure) (England) (Amendment) Order 2015

The proposal would not improve the economic, social and environmental conditions of the area nor does it comply with the development plan and therefore does not comprise sustainable development. There were no amendments to the scheme, or conditions which could reasonably have been imposed, which could have made the development acceptable and it was therefore not possible to approve the application. The Local Planning Authority has therefore implemented the requirement in Paragraphs 186-187 of the National Planning Policy Framework.

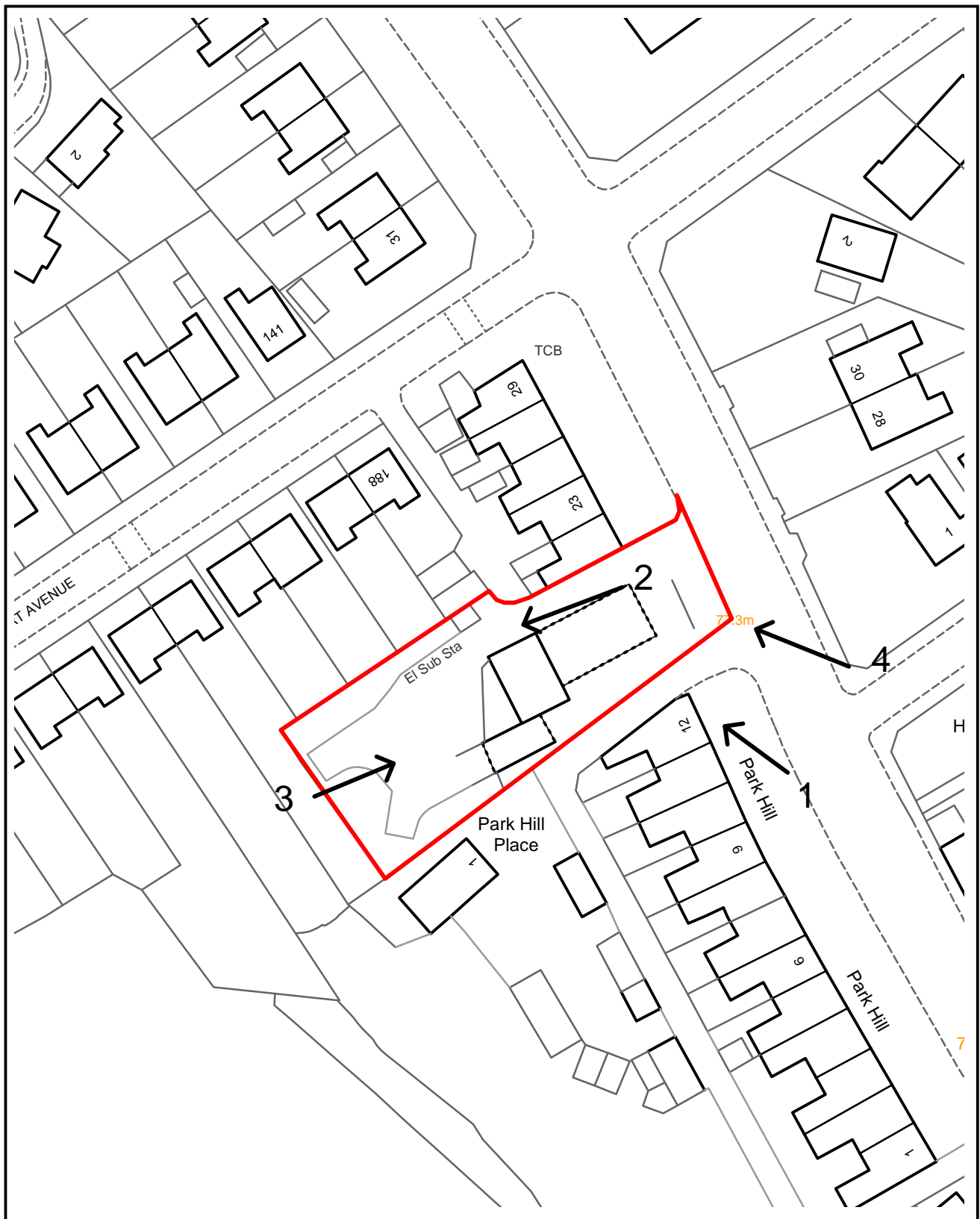
Recommendation: Refuse

Conditions/ Reasons

1. There is insufficient car parking provided within the site to comply with the Council's parking standards in respect of the proposed development. This would lead to vehicles parking and carrying out manoeuvres on the highway to the detriment of the free flow of traffic and highway safety. The proposed development therefore conflicts with Section 4 of the National Planning Policy Framework and the following Policies of the Bury Unitary Development Plan:
Policy EN1/1 - Townscape and Built Design
Policy EN1/7 - Throughroutes and Gateways
Policy HT2/4 - Car Parking and New Development
Policy HT4 - New Development
Policy CF1/1 - Location of New Community Facilities
Supplementary Planning Document 11 - Parking Standards in Bury.

For further information on the application please contact **Helen Longworth** on **0161 253 5322**

Viewpoints



PLANNING APPLICATION LOCATION PLAN

APP. NO 60045

ADDRESS: Former Texaco petrol station,
Bury Old Road, Prestwich

Planning, Environmental and Regulatory Services

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Bury
COUNCIL

60045

Photo 1



Photo 2

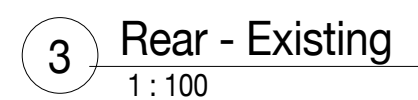
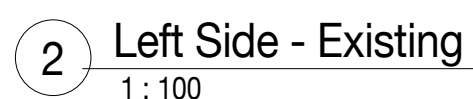
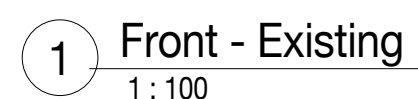


Photo 3



Photo 4

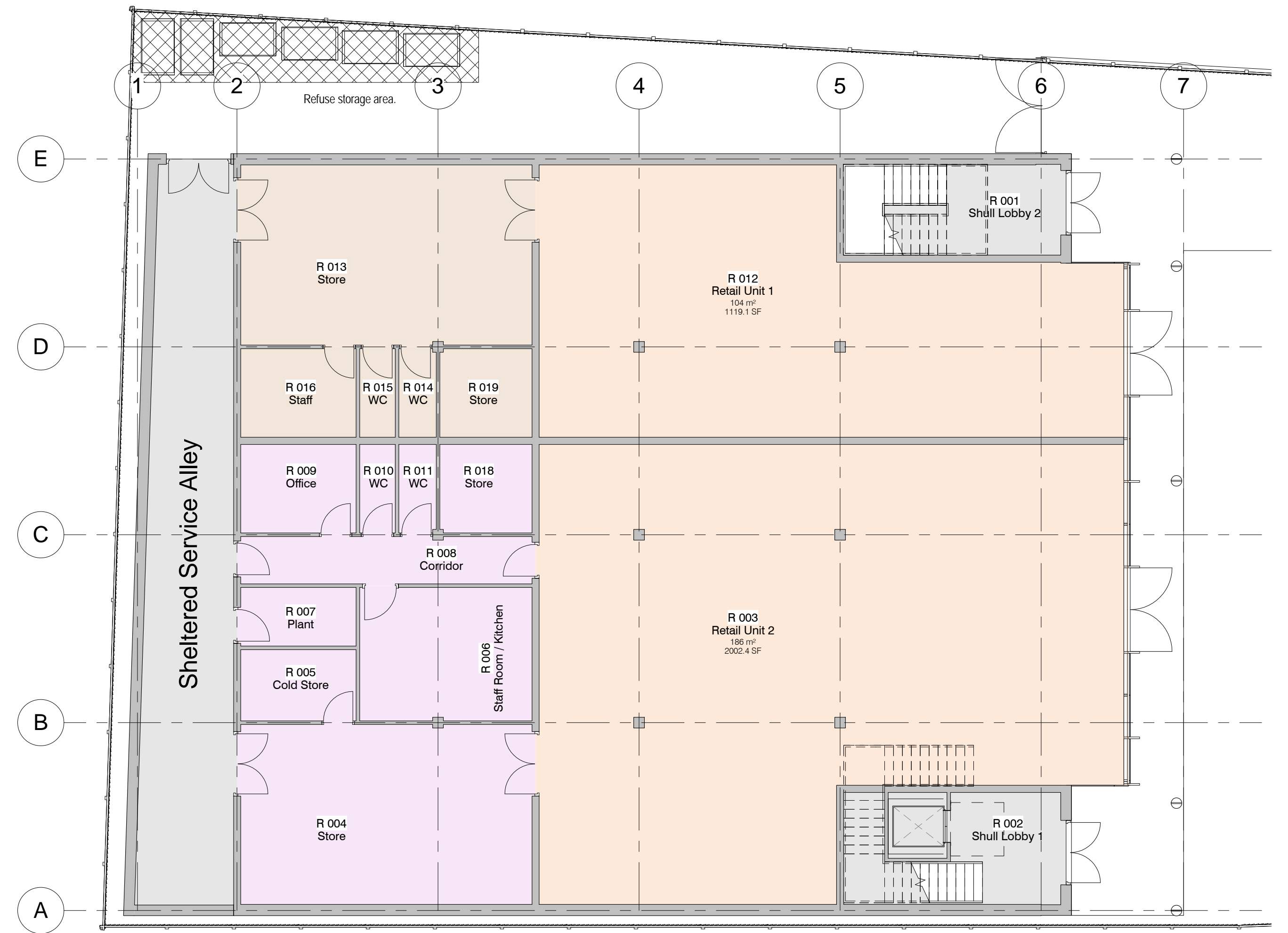




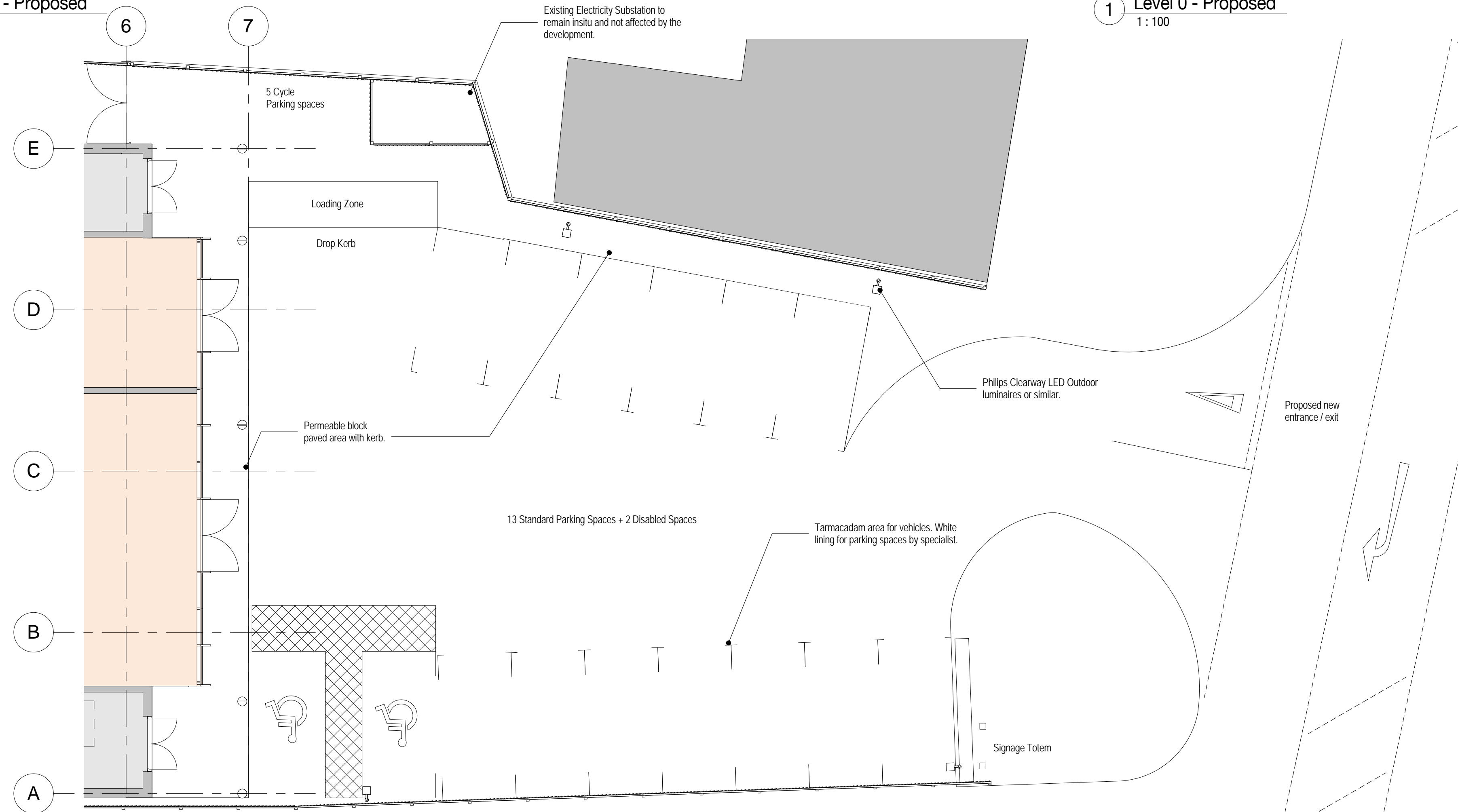
<h1>DEBTAL ARCHITECTURE</h1> <p>72 Bury New Road, Manchester, M20 0JL Tel: 0161 773 1030 www.debtalarchitecture.co.uk</p>			
CLIENT	Unimore Ltd		
PROJECT	Retail Units and Synagogue above		
TITLE	Existing GA Plans and Elevations		
PROJECT NO.	DISPOSAL NO.	REV.	
<h2>DA14079. 002.</h2>			
DATE	PLANNING		
ISSUED	DATE	OFFICE	
	02 FEB 2015		
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2 Level 1 - Proposed
1:100

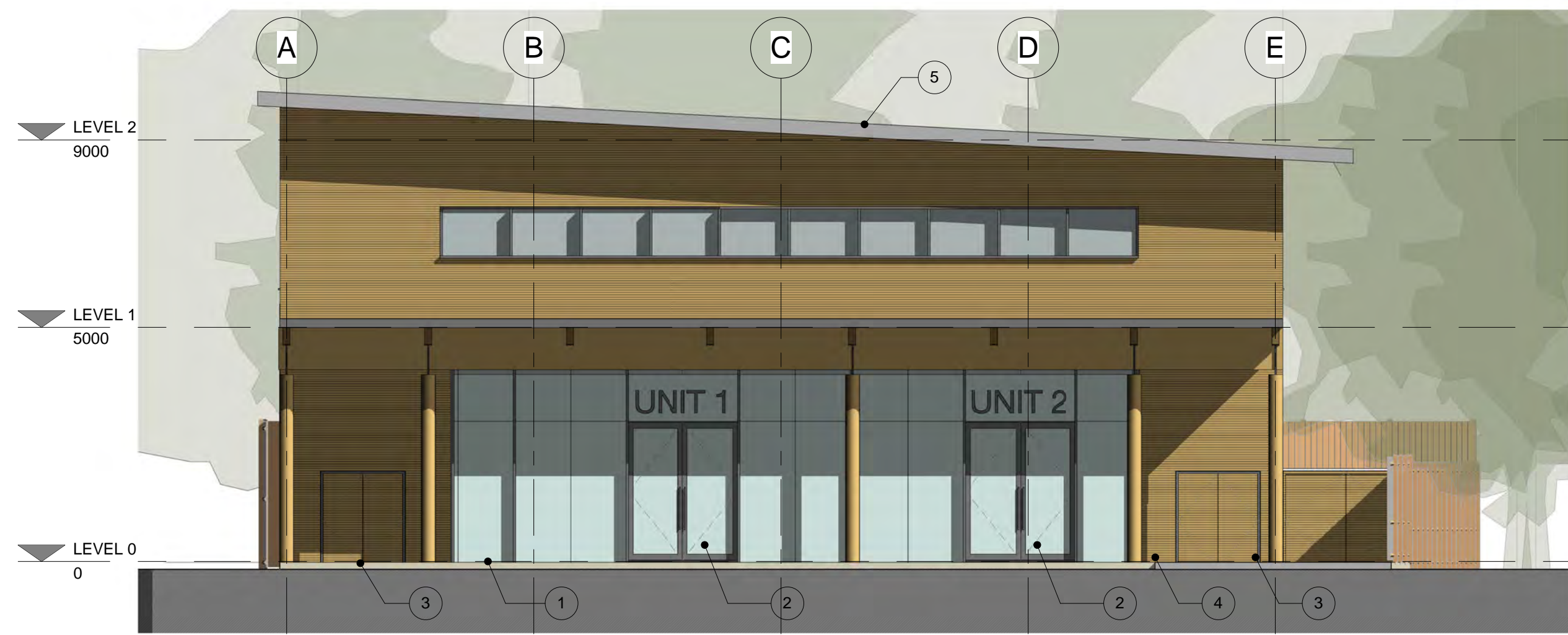


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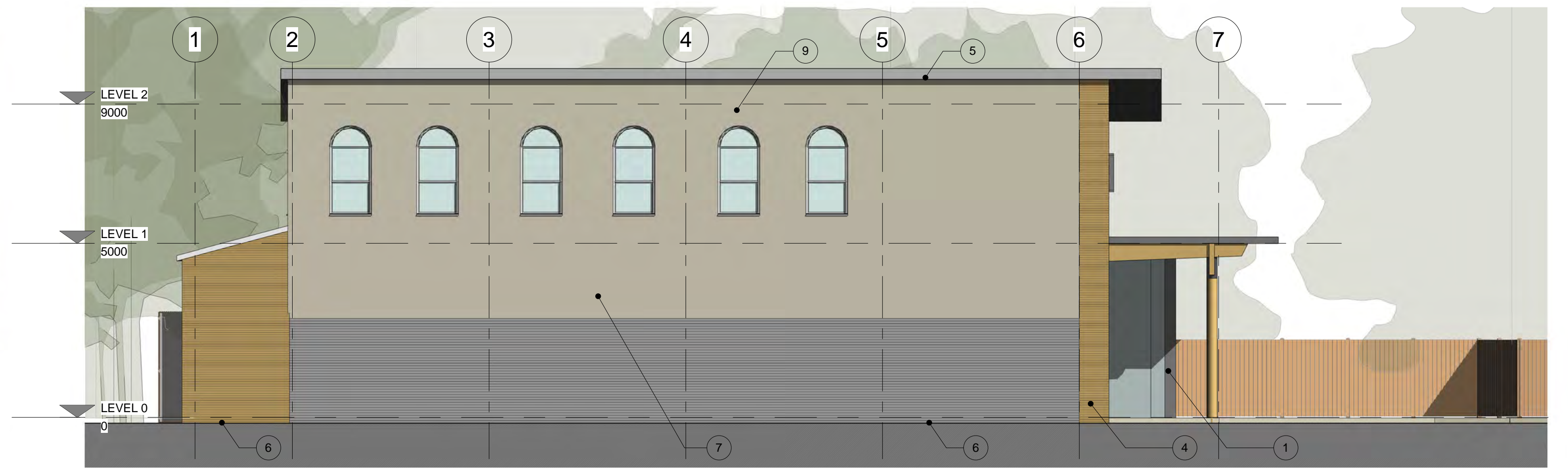


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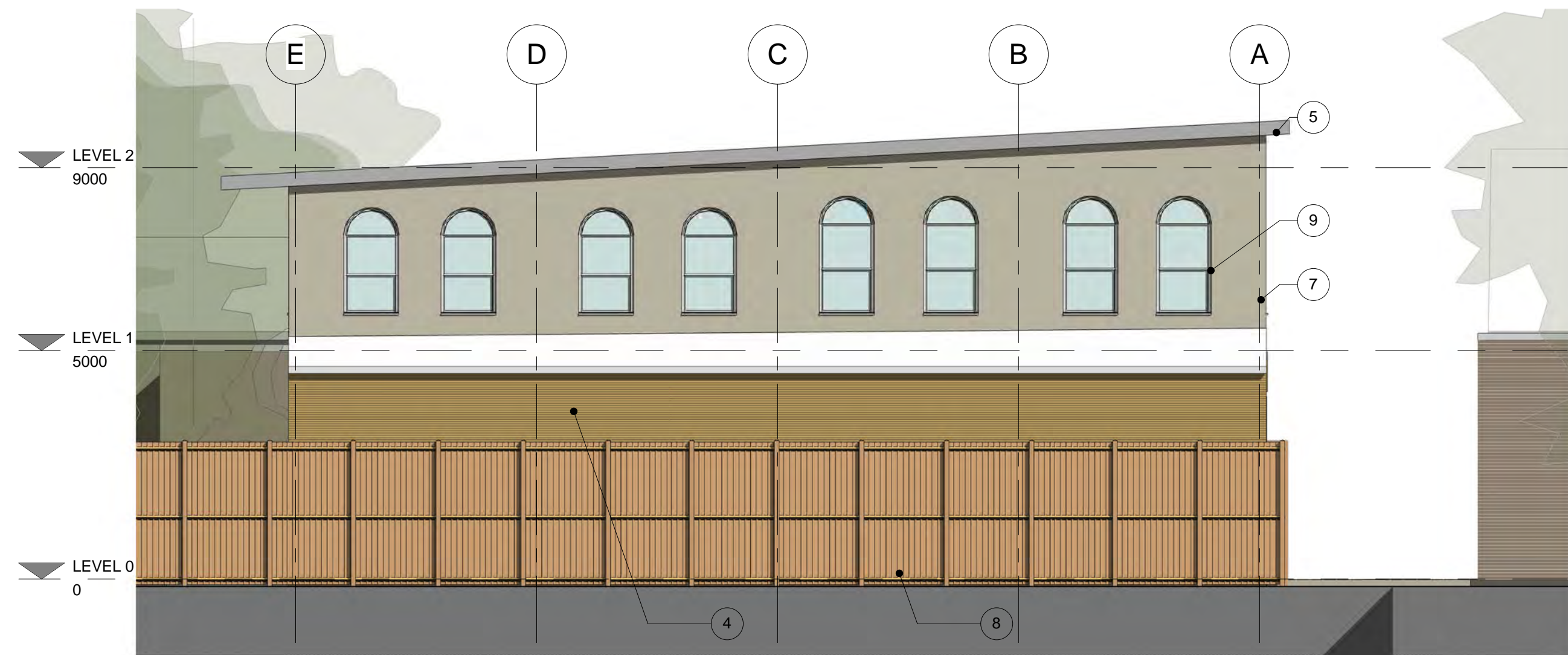
2	Site boundary amended. Exterior lighting spec indicated.	31/05/16	SP
1	Planning Amendments	05/05/16	SP
DEBTAL ARCHITECTURE			
72 Bury New Road, Manchester, M5 0JL office@debtalarchitecture.co.uk Tel: 0161 773 1830 www.debtalarchitecture.co.uk			
CLIENT Unimore Ltd			
PROJECT Retail Units and Synagogue above			
TITLE Proposed GA Plans			
PROJECT NO.	DA14079. 003.	REV.	2
DISCIPLINE PLANNING			
SCALE 1:100	DATE 17 FEB 2015	DESIGNED BY SP	
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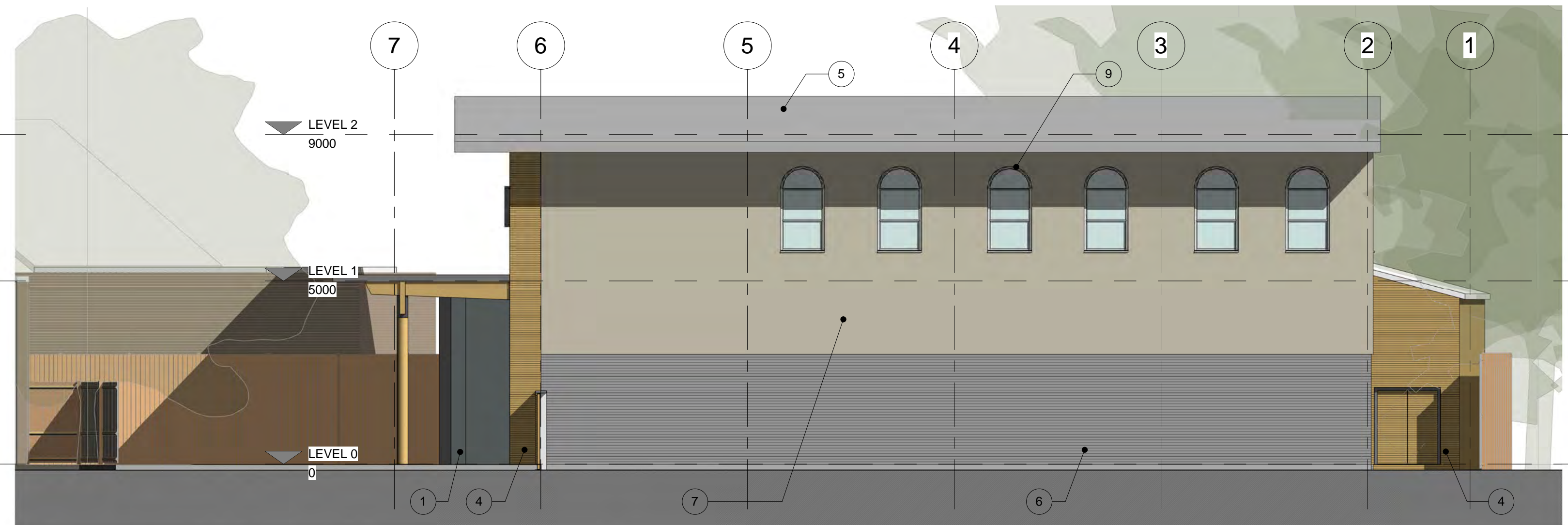
1 Front - Proposed
1 : 100



2 Left Side - Proposed
1 : 100



4 Rear - Proposed
1 : 100



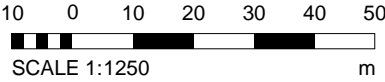
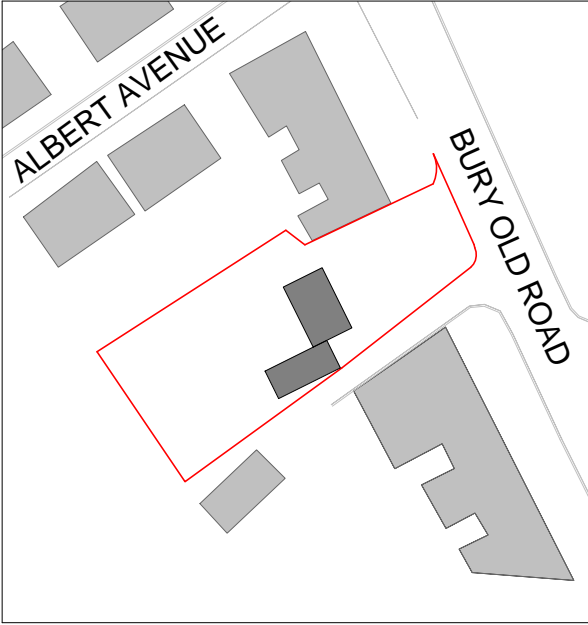
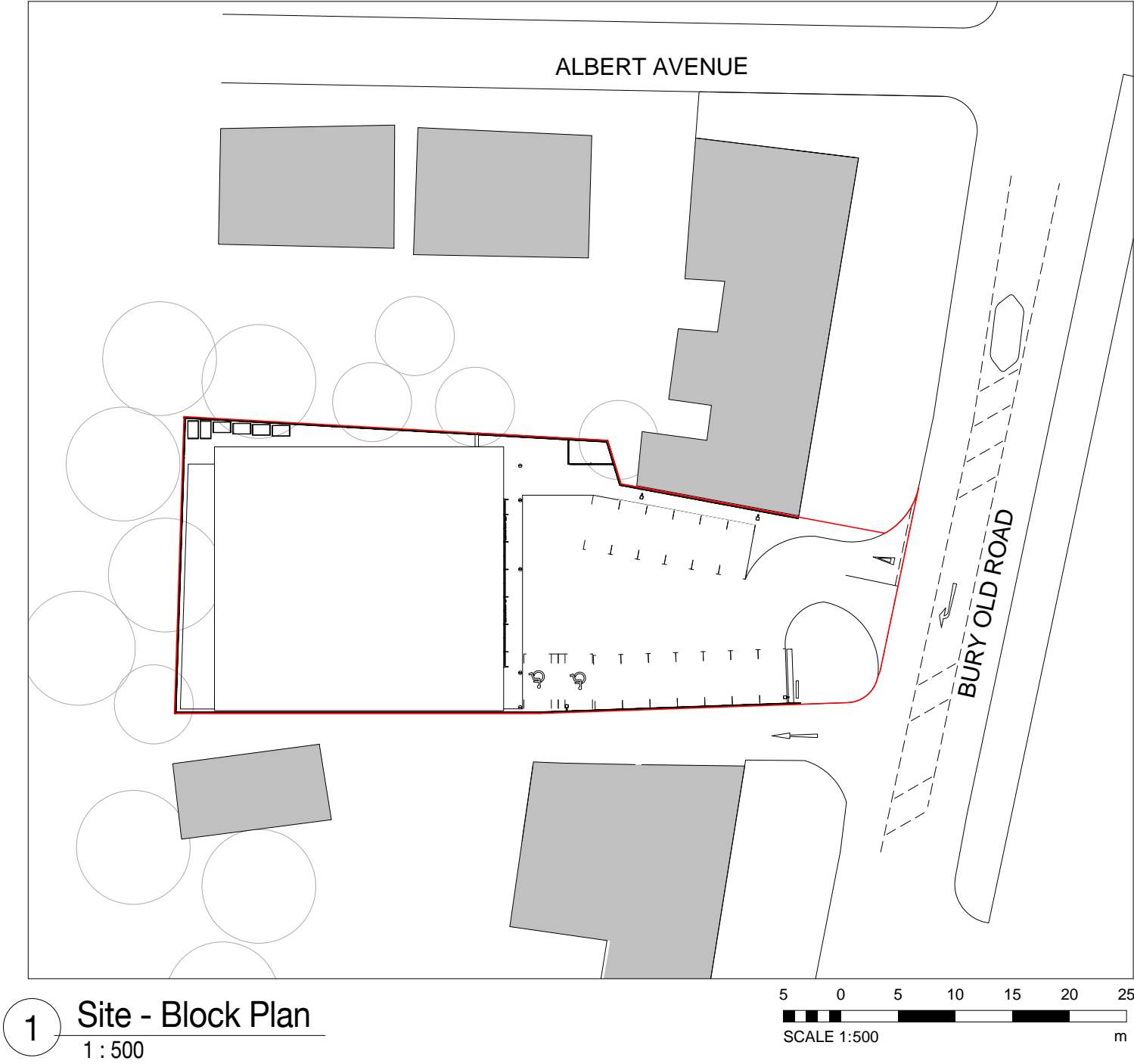
3 Right Side - Proposed
1 : 100



5 3D Axo - Proposed

- Material Schedule
- 1 Proposed Aluminium Curtain Walling with clear and obscured glazing
 - 2 Proposed Shopfront Entrance
 - 3 Proposed Synagogue Entrance
 - 4 Proposed Timber Cladding
 - 5 Proposed Roof
 - 6 Proposed Grey Brickwork
 - 7 Proposed Render
 - 8 Proposed Timber Fencing
 - 9 Proposed Obscured and Non-openable Windows

DEBTAL ARCHITECTURE		
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CLIENT	Unimore Ltd	
PROJECT	Retail Units and Synagogue above	
TITLE	Proposed GA Elevations	
PROJECT NO.	DA14079. 004.	REV.
DISCIPLINE	PLANNING	DATE
SCALE 1:1	1 : 100	DATE 02 FEB 2015
DRAWN BY	SP	CHECKED BY



N

2	Site boundary amended. Exterior lighting spec indicated.	19/05/16	SP
1	Planning Amendments	05/05/16	SP
<div>DEBTAL ARCHITECTURE</div> <div>72 Bury New Road, Manchester, M25 0JU. Tel: 0161 773 1630 office@debtalarchitecture.co.uk www.debtalarchitecture.co.uk</div>			
CLIENT Unimore Ltd			
PROJECT Retail Units and Synagogue above			
TITLE Site - Location and Block Plan			
PROJECT NO.			

Ward: Whitefield + Unsworth - Unsworth

Item 02

Applicant: Bury Strength and Conditioning Gym

Location: Unit 5, Croft Lane Industrial Estate, Croft Lane, Bury, BL9 8QG

Proposal: Change of use from light industrial (Class B1) and general industrial (Class B2) to personal training studio (Class D2)

Application Ref: 60527/Full

Target Date: 30/12/2016

Recommendation: Approve with Conditions

Description

This application relates to a former industrial unit within the existing industrial estate. The unit, with a footprint of 247sqm, is accessed from the rear/south side and is sandwiched between two larger industrial units. To the rear/south is a car park and service area and to the north is a car sales business that fronts onto Croft Lane. The nearest houses are positioned at the top of the banking on Haddon Close, to the rear. Other houses are across Croft Lane to the north.

The proposal is retrospective, having been operating since July 2016. It involves the creation of a gym within the unit with parking available in the existing car park. Opening hours would be

0600hrs - 2100hrs Monday to Friday

0900hrs - 1400hrs on Saturdays.

Sundays and Bank Holidays - Closed.

There would be 2 full time and two part-time staff. The only access point is from the car park on the south side of the building.

Relevant Planning History

60528 - 1 No. non-illuminated fascia sign (Sign 1) - Approved 15/12/2016

60528 - 1 no. non-illuminated estate sign (Sign 2); 1 no. non-illuminated roadside directional sign (Sign 3) - Refused 15/12/2016

Publicity

The following neighbours were notified by letter dated 14/11/16. 11,13 and 15 Haddon Close, 77, 96, 101, 111 - 129(odd) Croft Lane, Units 1 - 18 Croft Lane, Hollins Vale Works.

Two objections have been received. One from the occupiers of 13 Haddon Close and another from an unidentified address. Concerns are summarised below:

- Existing businesses operate between 8am and 8pm. The proposed earlier and later opening times would mean the increase in noise and disturbance.
- Opening and closing the steel gates at the start and end of the day would create additional noise.
- Noise from cars coming and going.
- Additional traffic would cause parking problems.

The objectors have been notified of the Planning Control Committee meeting.

Consultations

Traffic Section - No objection.

Environmental Health - No objection subject to conditions relating to noise attenuation.

Unitary Development Plan and Policies

EC2/2	Employment Land and Premises
EC4/1	Small Businesses
EN7/2	Noise Pollution
H3/1	Assessing Non-Conforming Uses
HT2/4	Car Parking and New Development
EN1/2	Townscape and Built Design

Issues and Analysis

The following report includes analysis of the merits of the application against the relevant policies of both the National Planning Policy Framework (NPPF) and the adopted Bury Unitary Development Plan (UDP) together with other relevant material planning considerations. The policies of the UDP that have been used to assess this application are considered to be in accordance with the NPPF and as such are material planning considerations. For simplicity, just the UDP Policy will be referred to in the report, unless there is a particular matter to highlight arising from the NPPF where it would otherwise be specifically mentioned.

Principle - The site is within an existing industrial estate but not within an Employment generating Area (EGA). Therefore UDP Policy EC2/2 Employment Land and Premises Outside Employment generating Areas is considered relevant. Under this Policy, development will only be allowed for business (B1), general industrial (B2) or warehousing (B8) uses. Other uses will be acceptable where they constitute limited development or where it can be demonstrated that the premises are no longer suitable for continued employment use and any new use would not detract from the area's character.

As the gym use falls within the D2 use class, Assembly and Leisure, and not within the Industrial uses normally acceptable within an industrial site, the onus is on the applicant to show that the proposed use is limited or that the premises are no longer suited to employment use.

The area of the building at 247sqm is not particularly large and within the wider industrial estate, would not represent a significant development. In addition, given that the building had been vacant for over a year before the current tenant took over, the retention of the new use would be generally acceptable. The proposed use would not be a 'sensitive use' such as residential, that could potentially prevent other businesses in the industrial estate from operating. Consequently, it is not considered that the change of use would substantially detract from the economic viability of the the rest of the industrial estate.

Furthermore, there would be little by way of alteration to the building so it could readily revert back to an employment use should the gym use cease. It is noted that the gym would employ two full time and two part time staff.

The proposal is therefore considered to be acceptable in principle, and considered would not detract from the area's employment generating value.

Residential Amenity and Noise - The garden boundary of the nearest neighbours on Haddon Close is approximately 30m away with the house being approximately 45m away on higher ground to the south.

As an industrial unit under Class B2, the premises would generate a certain amount of noise from processes that could be carried out, and which would be expected from such a location. The application unit itself could operate machinery or use equipment which would produce constant or sporadic noise on a daily basis, without the need for planning permission. Whilst the gym use may create a different type of noise, it is arguably no louder or aggravating than the noise some equipment or machinery may generate. Indeed, it could be argued it would be less noisy given the nature of the operation and the imposition of noise attenuation measures required by condition.

Environmental Health have not objected to the development but have recommended measures be put in place to mitigate noise outbreak eg. ensuring the main roller shutter door remains closed and a secondary cover used to further insulate from noise outbreak. A condition would be attached to any approval to ensure such measures are implemented. It is also noted that there is separate Environmental Health legislation that can control excessive noise at unsociable hours.

Given the location of the premises within an industrial setting, the noise which could potentially be generated from industrial processes, and appropriate conditions, it would not be reasonable in these circumstances to recommend refusal of the application on these grounds.

As such, the proposal is considered to be acceptable and would comply with UDP Policies EN7/2 - Pollution Control and EC4/1 - Small Businesses.

Visual amenity - No major physical alterations would be carried to the building, and its appearance as a typical industrial type unit would be retained.

Parking - 10 parking spaces would be provided within the existing park areas and service area adjacent to the premises. The nature and scale of the business is such that it would not generate significant amounts of traffic at one time. As such the parking available would be considered sufficient. As such, the proposed parking provisions are considered to be acceptable and would be in compliance with HT2/4 - Car Parking and New Development and SPD11.

Objections - The issue of noise and disturbance has been covered in the above report. Although the opening time is early, it is considered that noise and disturbance could be sufficiently mitigated by an hours condition and another that ensures the roller shutter remains closed and a further noise barrier is installed.

Statement in accordance with Article 35(2) Town and Country Planning (Development Management Procedure) (England) (Amendment) Order 2015

The Local Planning Authority worked positively and proactively with the applicant to identify various solutions during the application process to ensure that the proposal comprised sustainable development and would improve the economic, social and environmental conditions of the area and would accord with the development plan. These were incorporated into the scheme and/or have been secured by planning condition. The Local Planning Authority has therefore implemented the requirement in Paragraphs 186-187 of the National Planning Policy Framework.

Recommendation: Approve with Conditions

Conditions/ Reasons

1. This decision relates to the revised drawings received on 15/12/16 and the development shall not be carried out except in accordance with the drawings hereby approved.
Reason. For the avoidance of doubt and to ensure a satisfactory standard of design pursuant to the policies of the Bury Unitary Development Plan listed.
2. The gym use hereby permitted shall be operated only by Ms Natalie Epstein.
Reason. Permission has only been granted given the particular circumstances of the applicant and business pursuant to policies of the Unitary Development Plan listed below.
3. The roller shutter door on the main south elevation shall remain securely closed,

with the existing PVC cover in place, during hours of opening.

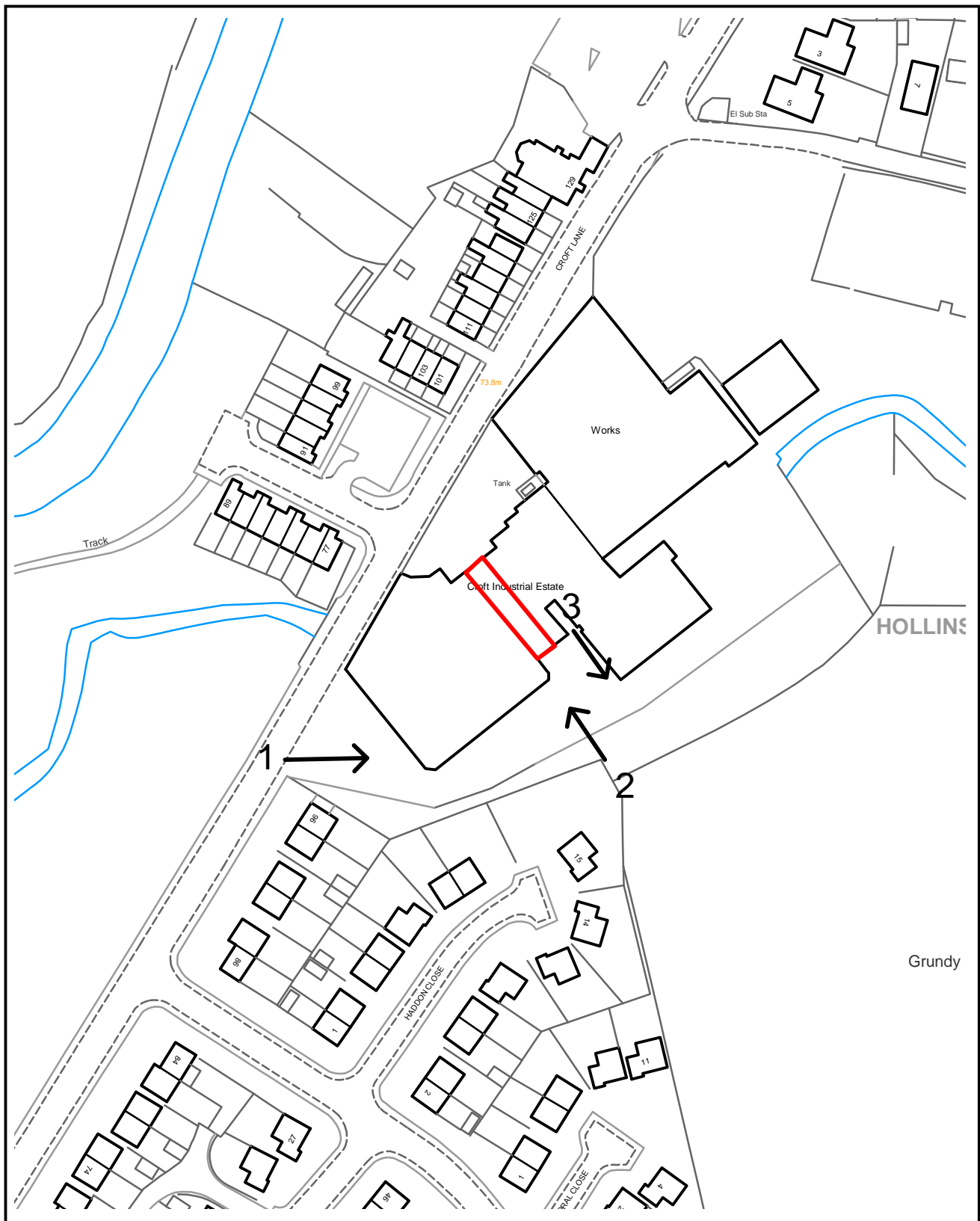
Reason. In order to reduce noise outbreak in the interests of residential amenity and pursuant to UDP Policy EC4/1 Small Businesses and EN7/2 Noise Pollution.

4. The gym shall not be open to customers outside the following hours:-
0600 hrs to 2100 hrs Monday to Friday
0900 hrs to 1400hrs Saturdays.
0900 hrs to 1200 hrs on Sundays and Bank Holidays.

Reason. To safeguard the amenities of the occupiers of nearby residential accommodation pursuant to Policies EC4/1 Small Businesses EN7/2 Noise Pollution.

For further information on the application please contact **Tom Beirne** on **0161 253 5361**

Viewpoints

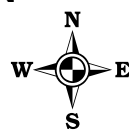


PLANNING APPLICATION LOCATION PLAN

APP. NO 60527

ADDRESS: Unit 5
Croft Lane Industrial Estate
Bury

Planning, Environmental and Regulatory Services 1:1250



Bury
COUNCIL

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60527

Photo 1



Photo 2



Photo 3



H. M. LAND REGISTRY

NATIONAL GRID PLAN

SD 8108

SD 8108

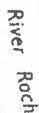
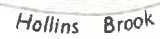
SECTION C

LANCASHIRE

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COUNTY OF GREATER MANCHESTER
JUDICIAL DISTRICT

ANSWER: DISTINCT



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11-70.

TITLE No. LA 232643

Ward: Prestwich - Holyrood

Item 03

Applicant: Mr Kingston

Location: Prestwich Heys A.F.C, Adie Moran Park, Sandgate Road, Whitefield, Manchester, M45 6WG

Proposal: Variation of conditions 4, 6 & 11 of planning permission 59412

Application Ref: 60567/Full

Target Date: 16/11/2016

Recommendation: Approve with Conditions

Description

Prestwich Heys FC is located on the eastern side of Sandgate Road and is protected recreational land. The football pitch is enclosed by concrete fencing and is located some 34 metres from the boundary with Sandgate Road. The area between the pitch and Sandgate Road is used as a car park and is unmarked. Within the compound there are a series of temporary structures, consisting of two large containers linked together for the changing rooms/toilet facility; one smaller container for storage and a large container for the clubhouse.

The M60 motorway forms the boundary to the north and there are residential properties to the west and south. There are playing fields and play equipment to the east of the site.

Planning consent was granted in November 2007 for various works to the site including:

- Retention of the clubhouse, changing rooms, toilets and store until 23 October 2012;
- Installation of 6 floodlighting units;
- Retention of the concrete panel fencing; and
- Resurfacing and landscaping of the car park.

Permission was granted in June 2012 (55178) for the retention of the changing rooms, store, toilets and clubhouse until 31 December 2015 to allow the Club time to progress the replacement buildings and to improve the ground and existing facilities in order to meet the minimum standards of the Football Association.

Permission was granted in January 2016 (59412) for the variation of conditions 2 and 5 to allow the retention of the changing rooms, store, toilets and clubhouse until 31 December 2018. The extension of time was required to allow the club time to progress its plans, obtain funding for new facilities to meet the minimum standards of the Football Association.

The applicant seeks permission for the removal of conditions 4 (samples of timber cladding for the fencing), 6 (landscaping scheme) and 11 (demarcation of the car parking) of permission 59412.

The application seeks the retention of the existing fencing on the perimeter of the site (condition 4) and the retention of the existing vegetation on site and additional planting between the sub station and the vehicular access gate (condition 6). The current surfacing cannot be demarcated and as such, the club would ensure parking stewards would be available for each match to align the vehicles on the car park (condition 11).

Relevant Planning History

46044 - Retrospective application for temporary developments including car park, portable contractors office units and holding compound at Prestwich Heys FC, Sandgate Road, Whitefield. Approved with conditions - 26 July 2006.

47075 - Football club cabins, toilets and clubhouse, fencing and lighting pylons and retention of existing fencing at Prestwich Heys FC, Sandgate Road, Whitefield. Withdrawn (Invalid) - 2 January 2007.

47511 - Retention of changing rooms, store, clubhouse and fencing and installation of 6 floodlighting units at Prestwich Heys FC, Sandgate Road, Whitefield. Refused - 31 May 2007

48290 - Retention of changing rooms, store, toilets, clubhouse and fencing; installation of 6 floodlighting units including resurfacing and landscaping of car park at Prestwich Heys FC, Sandgate Road, Whitefield. Approved with conditions - 21 November 2007.

55178 - Prior notification for installation of 15 metre high streetpole with 6 no. antennas and equipment cabinets to facilitate site sharing at Prestwich Heys FC, Sandgate Road, Whitefield. Prior approval required and granted - 19 June 2012.

55197 - Variation of condition 6 on planning permission 48290 to allow the retention of changing rooms, store, toilets and clubhouse until 31 December 2015 at Prestwich Heys FC, Sandgate Road, Whitefield. Approved with conditions - 26 June 2012.

58111 - Installation of modular steel changing room at Prestwich Heys FC, Sandgate Road, Whitefield. - Approve with Conditions 18 December 2014.

59412 - Variation of conditions 3 and 5 on planning permission 55197 to allow the retention of changing rooms, store, toilets, floodlights and clubhouse until 31 December 2018 at Prestwich Heys FC, Sandgate Road, Whitefield. Approved with conditions - 4 January 2016.

Publicity

The neighbouring properties were notified by means of a letter on 22 September 2016.

Two letters have been received, which have raised the following issues:

- No pre-commencement conditions have been carried out.
- The floodlights were supposed to be retracted after each game and are only retracted for maintenance.
- Since the initial wall was erected, they have not complied with any planning stipulations.
- Disappointed that with the clubs use of the media, the club feel justified in that the Council should drop the conditions that were so important to the residents at the Council meeting.
- Residents have been blighted with graffiti and there has been no effort to maintain this, which can be seen from the motorway.
- The club only concentrate on what's inside the club with no regard to the outside. Gates are left open and we are plagued by motorcross due to the open gates.
- The erection of lights makes a total mockery of the planning process and gives the impression that you can do what you want despite what the residents feel.

The objectors have been notified of the Planning Control Committee meeting.

Consultations

None required.

Unitary Development Plan and Policies

NPPF	National Planning Policy Framework
RT1/1	Protection of Recreation Provision in the Urban Area
RT1/2	Improvement of Recreation Facilities
EN1/2	Townscape and Built Design
EN7	Pollution Control
EN1/5	Crime Prevention
H3/1	Assessing Non-Conforming Uses
HT5/1	Access For Those with Special Needs

Issues and Analysis

The following report includes analysis of the merits of the application against the relevant policies of both the National Planning Policy Framework (NPPF) and the adopted Bury Unitary Development Plan (UDP) together with other relevant material planning considerations. The policies of the UDP that have been used to assess this application are considered to be in accordance with the NPPF and as such are material planning considerations. For simplicity, just the UDP Policy will be referred to in the report, unless there is a particular matter to highlight arising from the NPPF where it would otherwise be specifically mentioned.

Impact upon the surrounding area

Retention of fencing - The existing fencing is located on the perimeter of the site and would be appropriate in terms of design. The retention of the existing fencing would not be unduly visible to the surrounding properties due to the presence of the existing vegetation, both immediately adjacent to the fence and along the boundary with Sandgate Road. This vegetation is to be retained and additional planting will be provided between the substation and the vehicular access gate along Sandgate Road, which would screen the fencing from view. As such, the proposed development would not be a prominent feature within the locality and would be in accordance with Policy EN1/2 of the Bury Unitary Development Plan.

Parking - The previous approval sought to provide 79 marked out parking spaces within the existing car parking. The existing car park is surfaced with a gravel type material, which would not be suitable for demarcation. The applicant has confirmed that there would be parking stewards present on each match day, who would align the vehicles within the parking areas, thereby ensuring an efficient use of the space and providing 79 car parking spaces. Therefore, the proposed development would provide a suitable level of parking provision and would be in accordance with HT2/4 of the Bury Unitary Development Plan and SPD 11.

Response to objectors

The issues raised by the objectors relate to works, which are not associated with this application. The concerns will be passed to the enforcement section for investigation.

Statement in accordance with Article 35(2) Town and Country Planning (Development Management Procedure) (England) (Amendment) Order 2015

The Local Planning Authority worked positively and proactively with the applicant to identify various solutions during the application process to ensure that the proposal comprised sustainable development and would improve the economic, social and environmental conditions of the area and would accord with the development plan. These were incorporated into the scheme and/or have been secured by planning condition. The Local Planning Authority has therefore implemented the requirement in Paragraphs 186-187 of the National Planning Policy Framework.

Recommendation: Approve with Conditions

Conditions/ Reasons

1. The floodlighting units shall not be installed unless and until the football ground alterations including the car park works, fencing cladding with removal of cranked post tops and all associated landscaping works have been completed to the written satisfaction of the Local Planning Authority.
Reason: In order to safeguard the visual amenities of the area pursuant to Policy EN1/2 - Townscape and Built design of the Bury Unitary Development Plan.
2. The floodlights shall not be illuminated on more than two occasions per calendar week and they shall not be illuminated after 2130 hours on any day from Monday to Friday and not after 1830 hours on Saturdays or Sundays.
Reason: In order to protect the residential amenities of the area pursuant to Policy EN1/2 - Townscape and Built Design and Policy EN7 - Pollution Control of the Bury Unitary Development Plan.
3. The position and angle of the floodlights shall at all times accord with the lighting contours and lighting details as indicated on drawing UKS4119 prepared by Abacus Lighting Ltd. The floodlighting shall also be carried out and operated fully in accordance with the submitted particulars by Abacus Lighting Ltd. Should any alteration to the lighting be proposed, details shall be submitted to and approved by the Local Planning Authority prior to that alteration being carried out.
Reason: In order to prevent light pollution in accordance with Policy EN7 - Pollution Control of the Bury Unitary Development Plan.
4. The existing fencing shall be retained and repaired and replaced when required.
Reason: In the interests of visual amenity and to ensure a satisfactory development pursuant to Policy EN1/2 - Townscape and Built Design of Bury Unitary Development Plan.
5. The permission hereby granted for the retention of the changing rooms, store, toilets and clubhouse is for a limited period only, namely for a period expiring on 31 December 2018, and these buildings are required to be removed at the expiration of the said period and the land reinstated within one month of their removal to its former condition to the written satisfaction of the Local Planning Authority.
Reason: The buildings are of a temporary nature only.
6. Notwithstanding the details shown on drawing ref. C1369-P03, a landscaping scheme for the area between the electricity substation and the vehicular access gate on Sandgate Road shall be submitted to and approved in writing by the Local Planning Authority within 1 month of the date of this decision.
The approved details shall be implemented not later than 12 months from the date of the decision and the existing vegetation shall be retained. Any trees or shrubs removed, dying or becoming severely damaged or becoming severely diseased within five years of planting shall be replaced by trees or shrubs of a similar size or species to those originally required to be planted to the written satisfaction of the Local Planning Authority.
Reason: To secure the satisfactory development of the site and in the interests of visual amenity pursuant to Policy EN1/2 - Townscape and Built Design and EN8/2 – Woodland and Tree Planting of the Bury Unitary Development Plan.
7. Any soil or soil forming materials brought to site for use in soft landscaping, filling and level raising shall be tested for contamination and suitability for use on site. Proposals for contamination testing including testing schedules, sampling frequencies and allowable contaminant concentrations (as determined by appropriate risk assessment) and source material information shall be submitted to and approved in writing by the Local Planning Authority prior to works commencing on site, and;
The approved contamination testing shall then be carried out and validity evidence (laboratory certificates etc) submitted to and approved in writing by the Local Planning Authority prior to any soil or soil forming materials being brought

onto site.

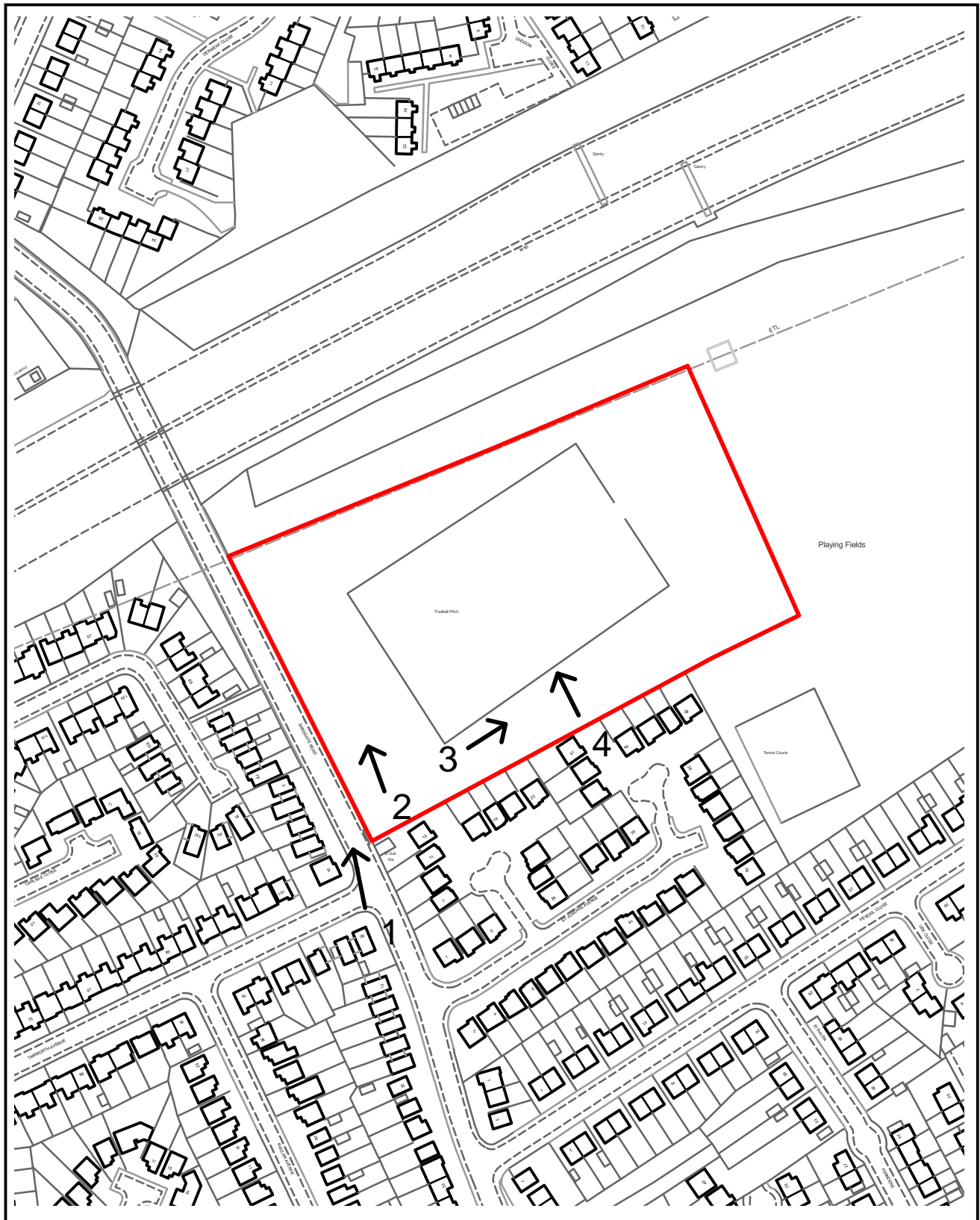
Reason - To secure the satisfactory development of the site in terms of human health and the wider environment and pursuant to Section 11 of the National Planning Policy Framework.

8. If during any works on site, contamination is suspected or found, or contamination is caused, the Local Planning Authority shall be notified immediately. Where required, a suitable risk assessment shall be carried out and/or any remedial action shall be carried out in accordance to an approved process and within approved timescales to the approval of the Local Planning Authority.
Reason - To secure the satisfactory development of the site in terms of human health and the wider environment and pursuant to Section 11 of the National Planning Policy Framework.
9. External lighting shall be installed in order to illuminate the route from the disabled parking spaces to the ground entrance and no development of the car park shall take place unless and until the details of this lighting have been submitted to and approved in writing by the Local Planning Authority. The development shall not be carried out other than in accordance with the approved details.
Reason: In order to ensure that the development would facilitate ease of access for those with special needs in accordance with Policy HT5/1 - Access for Those with Special Needs of the Bury Unitary Development Plan.
10. The ramped access to the clubroom shall be carried out fully in accordance with the details shown on the submitted plan ref. C1369-P05.
Reason: In order to ensure that the development would facilitate ease of access for those with special needs in accordance with Policy HT5/1 - Access for Those with Special Needs of the Bury Unitary Development Plan.
11. The development hereby approved shall be carried out in accordance with the supporting statement in relation to car parking, dated 22 September 2016 and parking stewards shall be available for all match days.
Reason. To ensure adequate off street car parking provision in the interests of road safety, amenity and of those with special needs pursuant to policies HT2/4 - Car Parking and New Development and HT5/1 - Access for Those with Special Needs of the Bury Unitary Development Plan.
12. No lighting source shall be directly visible to drivers on the M60 Motorway.
Reason: In the interests of highway safety and to ensure that the M60 continues to operate effectively as part of the national system of routes for through traffic as set out in Section 10(2) of the Highways Act 1980.
13. The lighting proposed shall not cause a glare problem to motorists on the M60 motorway.
Reason: In the interests of highway safety and to ensure that the M60 continues to operate effectively as part of the national system of routes for through traffic as set out in Section 10(2) of the Highways Act 1980.
14. Rear baffles shall be installed to the floodlights on the three units situated on the side of the football ground that is nearest to the houses in St. Joseph's Avenue.
Reason: In order to minimise any impact from spill light on nearby residential properties in accordance with Policy EN7 - Pollution Control of the Bury Unitary Development Plan.

15. This decision relates to drawings numbered C1369-P01, C1369-P02, C1369-P03, C1369-P04, C1369-P05, UKS4119 and, subject to condition 7, the development shall not be carried out except in accordance with the drawings hereby approved. Reason. For the avoidance of doubt and to ensure a satisfactory standard of design pursuant to policies of the Bury Unitary Development Plan listed below.

For further information on the application please contact **Helen Longworth** on **0161 253 5322**

Viewpoints

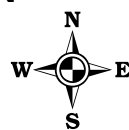


PLANNING APPLICATION LOCATION PLAN

APP. NO 60567

ADDRESS: Prestwich Heys AFC
Adie Moran Park
Sandgate Road

Planning, Environmental and Regulatory Services
Whitefield



Bury
COUNCIL

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60567

Photo 1



Photo 2



Photo 3



Photo 4





Aerial Photo

60567

Prestwich Heys A.F.C Aidie Moran Park
Sandgate Road, Whitefield



Bury
COUNCIL

Ward: Bury West - Church

Item 04

Applicant: Mr S Ali

Location: 29 Bolton Road, Bury, BL8 2AB

Proposal: Change of use from A1 (retail) to A5 (hot food takeaway) with addition of flue to rear elevation

Application Ref: 60688/Full

Target Date: 05/01/2017

Recommendation: Approve with Conditions

Description

The application relates to a ground floor shop and ancillary accommodation located on Bolton Road. The premises is located within a row of terraced shops that front Bolton Road. The area is commercial in character, however there are some first floor flats located above the shops within the terrace.

Planning permission is sought to convert the ground floor into a hot food takeaway. The first floor accommodation will be retained as ancillary to the takeaway providing a staff area and store. Alterations to the front include the provision for a timber fascia sign above the existing shop window and entrance door. The hours proposed are 1100-0100.

Relevant Planning History

None

Publicity

Letters sent to 21 properties on 15/11/2016 - One objection received from No. 31 Bolton Road with concerns regarding:

- Noise and disturbance caused by a takeaway operating until 1 am.
- Will increase the existing rat problem in the area.
- Parking issues - will block the side street next to this property.

Consultations

Traffic Section - No objection

Environmental Health - Commercial Section and Pollution Control - No objection subject to condition requiring a scheme for treating, diluting and dispersing fumes and odours.

Unitary Development Plan and Policies

EC2/1	Employment Generating Areas
S1/4	Local Shopping Centres
EN1/2	Townscape and Built Design
HT2/4	Car Parking and New Development
S2/6	Food and Drink

Issues and Analysis

The following report includes analysis of the merits of the application against the relevant policies of both the National Planning Policy Framework (NPPF) and the adopted Bury Unitary Development Plan (UDP) together with other relevant material planning considerations. The policies of the UDP that have been used to assess this application are considered to be in accordance with the NPPF and as such are material planning considerations. For simplicity, just the UDP Policy will be referred to in the report, unless there is a particular matter to highlight arising from the NPPF where it would otherwise be

specifically mentioned.

Policy

Policy S2/6 - Food and Drink and states that in considering proposals, regard be given to the following factors:

- the amenity of nearby residents by reason of noise, smell, litter and opening hours;
- whether or not the proposal would result in an over concentration of these types of uses;
- parking and servicing provision and traffic generation;
- provision for the storage and disposal of refuse and customer litter;
- environmental impact of any ventilation flues and/or ducting.

Policy S1/4 - Local Shopping Centre seeks to maintain and enhance local shopping centres and will encourage the provision of a range of shopping facilities required to serve purely local needs.

Policy EN1/2 - Townscape and Built Design seeks to ensure that any proposals would not have an adverse impact on the particular character and townscape of an area.

Policy HT2/4 - Car Parking and New Development requires all applications for development to make adequate provision for their car parking and servicing requirements.

Principle

Whilst there are other hot food takeaways in the Local Shopping Centre, there is only one other within this row of shops (No. 27 - 45). In terms of shop frontage, 80% would be maintained in A1 use, with the remaining 20% in A5 use. As such, it is considered there would not be an over concentration of A5 uses and would comply with Policy S2/6 - Food and Drink.

Residential amenity

Although there may be some residential accommodation above the adjacent shops, the area is commercial in character and fronts onto a busy dual carriageway into Bury Town Centre. It is therefore considered that there is already background activity in and around the area. There are other takeaways and uses in the locality which are open late in the evening and as such the addition of one other premises with evening opening hours is considered not to have a significant impact on amenity in the area.

The application is therefore considered to comply with Policy S2/6 - Food and Drink in this respect.

Visual amenity

The flue would be located on the rear roof of the premises and not visible from the public domain. The front elevation remains relatively unchanged with the shopfront maintained, with an area of signage identified above the window. There would be no other physical alterations to the building which would affect the visual amenity of the area.

In this instance therefore the application is considered to comply with Policy EN1/2 - Townscape and Built Design.

Parking

There is already restricted parking along the entire row of shops fronting the Bolton Road Shopping Centre. A hot food takeaway is a use where customers tend to park for short periods of time in order to pick up food. There are a number of side streets in the locality where there is short stay parking and no objections have been raised from the Traffic Section in relation to the proposal. As such, in this locality and given the existing situation, the proposal is considered to be acceptable and complies with Policy S2/6 - Food and Drink and Policy HT2/4 - Car Parking and New Development.

Bin storage and servicing

There would be a dedicated bin store area to the rear of the shop which would be accessed

via Albion Court for waste collection. The application is therefore considered to comply with Policy S2/6 - Food and Drink in this respect.

Neighbour Objection

It is considered that the impact of the proposal on residential amenity and parking has been assessed above. The proposed opening hours have been restricted by condition to midnight.

Matters controlled by non-planning legislation eg pest control is a non-material planning consideration not relevant to the decision.

Statement in accordance with Article 35(2) Town and Country Planning (Development Management Procedure) (England) (Amendment) Order 2015

The Local Planning Authority worked positively and proactively with the applicant to identify various solutions during the application process to ensure that the proposal comprised sustainable development and would improve the economic, social and environmental conditions of the area and would accord with the development plan. These were incorporated into the scheme and/or have been secured by planning condition. The Local Planning Authority has therefore implemented the requirement in Paragraphs 186-187 of the National Planning Policy Framework.

Recommendation: Approve with Conditions

Conditions/ Reasons

1. The development must be begun not later than three years beginning with the date of this permission.
Reason. Required to be imposed by Section 91 Town & Country Planning Act 1990.
2. This decision relates to drawings numbered ARSH/02 Dwg 01, ARSH/02 Dwg 03, ARSH/02 Dwg 02A Amendment A and 0085-25-01 and the development shall not be carried out except in accordance with the drawings hereby approved.
Reason. For the avoidance of doubt and to ensure a satisfactory standard of design pursuant to the policies of the Bury Unitary Development Plan listed.
3. No development shall commence unless and until a scheme for treating, diluting and dispersing fumes and odours has been submitted to and approved in writing by the Local Planning Authority, which shall include:
 - a written statement from a suitably qualified person that is a member of the Heating and Ventilation Contractors Association (HVCA), which demonstrates compliance with the measures proposed in the Guidance on the Control of Odour and Noise from Commercial Kitchen Exhaust Systems :DEFRA 2005 (or if applicable such superseding guidance as shall prevail at the time of commencement of the development); and
 - the relevant manufacturer and installer instructions for any associated equipment with details of maintenance requirements .

The scheme as approved shall be implemented, available for use and maintained in accordance with the approved scheme whilst it shall serve the development.

Reason. To protect the residential amenities of nearby residential property from impact upon from fumes and odour pursuant to UDP Policy S2/6 - Food and Drink.

4. The installation shall be so designed such that the maximum noise emitted does not exceed NR25 (Noise Rating) in the bedrooms of the nearest residential property, with the windows of that residential property being open in the normal manner for ventilation purposes.

The scheme as approved shall be fully implemented and all equipment installed shall be used and maintained in accordance with the manufacturers and installers instructions.

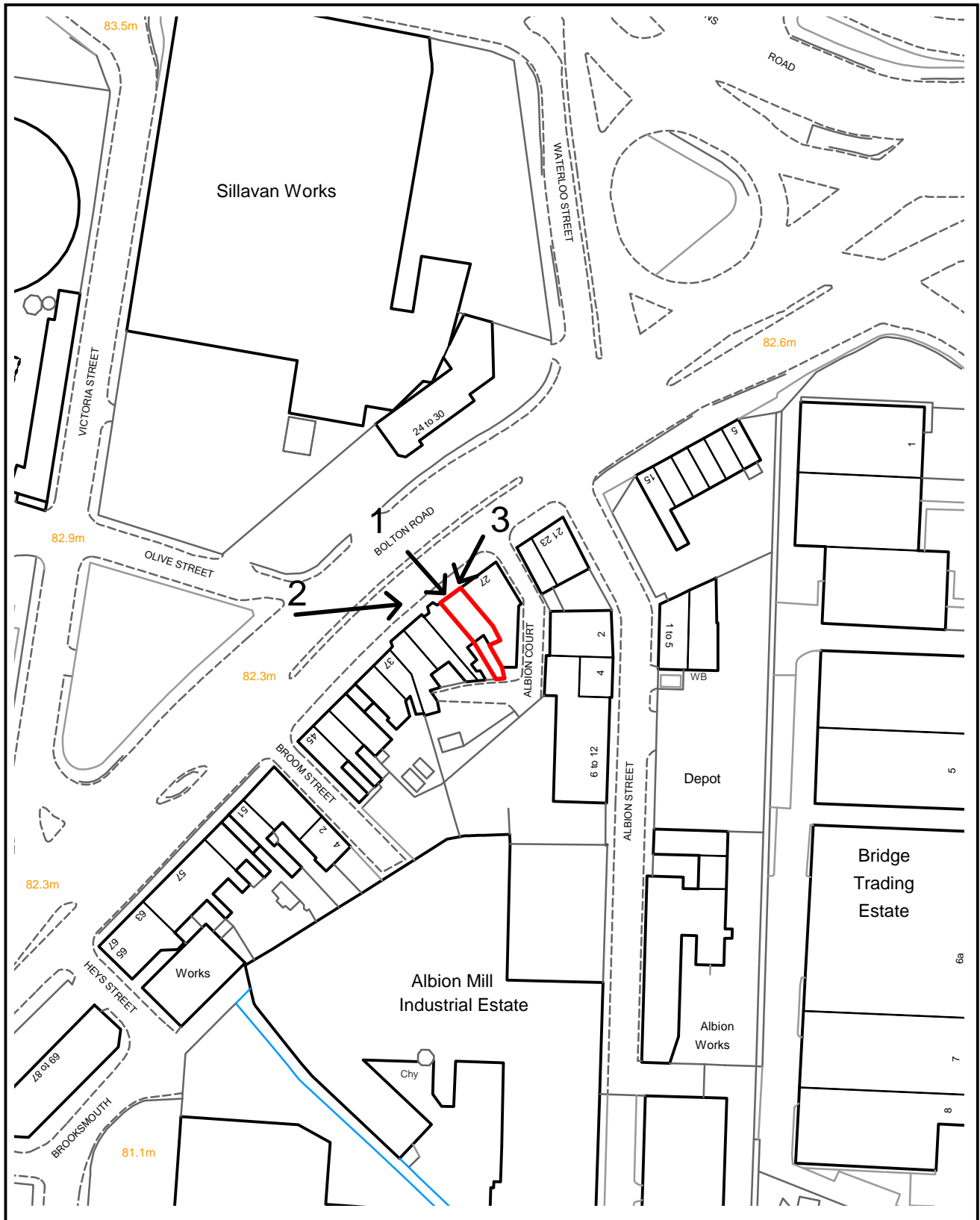
Reason. To protect the residential amenities of nearby residential property from impact upon from fumes and odour pursuant to UDP Policy S2/6 - Food and Drink.

5. The use hereby permitted shall not be open to customers outside the following times: 11.00 to 00:00 Daily.

Reason. To safeguard the amenities of the occupiers of nearby residential accommodation pursuant to Policies S2/5 – New Local Shopping Provision Outside Recognised Shopping Centres and S2/6 – Food and Drink of the Bury Unitary Development Plan.

For further information on the application please contact **Helen Goldsbrough** on **0161 253 5277**

Viewpoints



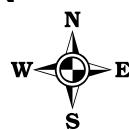
PLANNING APPLICATION LOCATION PLAN

APP. NO 60688

**ADDRESS: 29 Bolton Road
Bury**

Planning, Environmental and Regulatory Services

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Bury
COUNCIL

60688

Photo 1

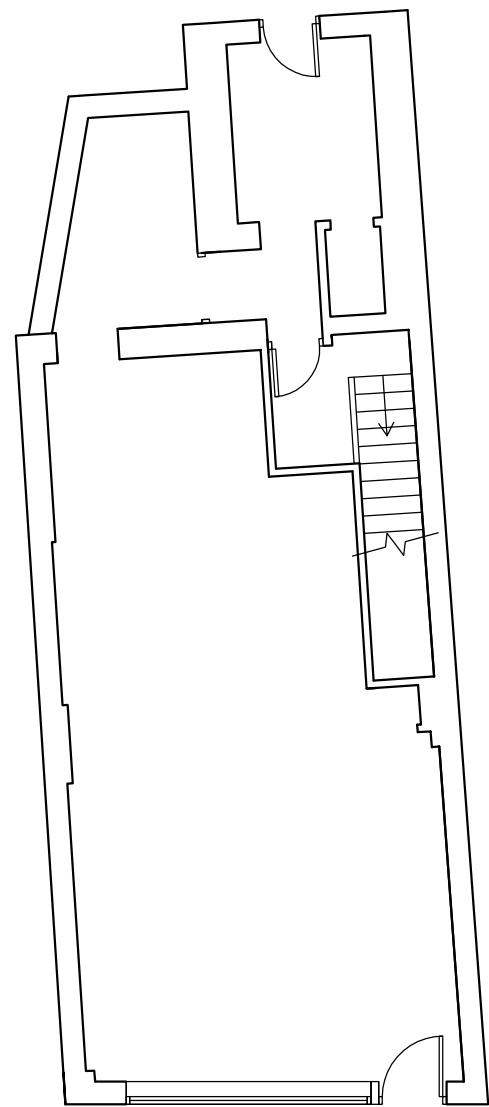


Photo 2

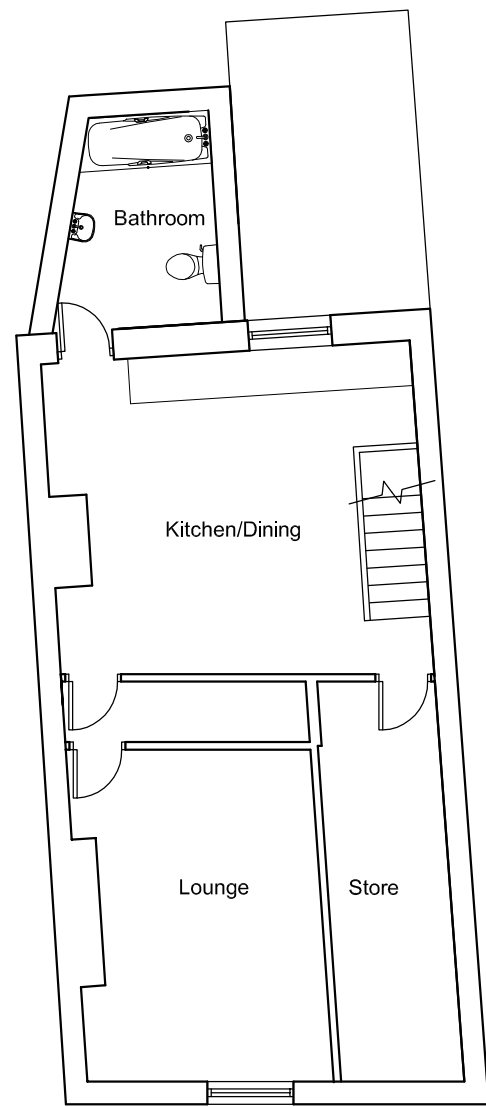


Photo 3

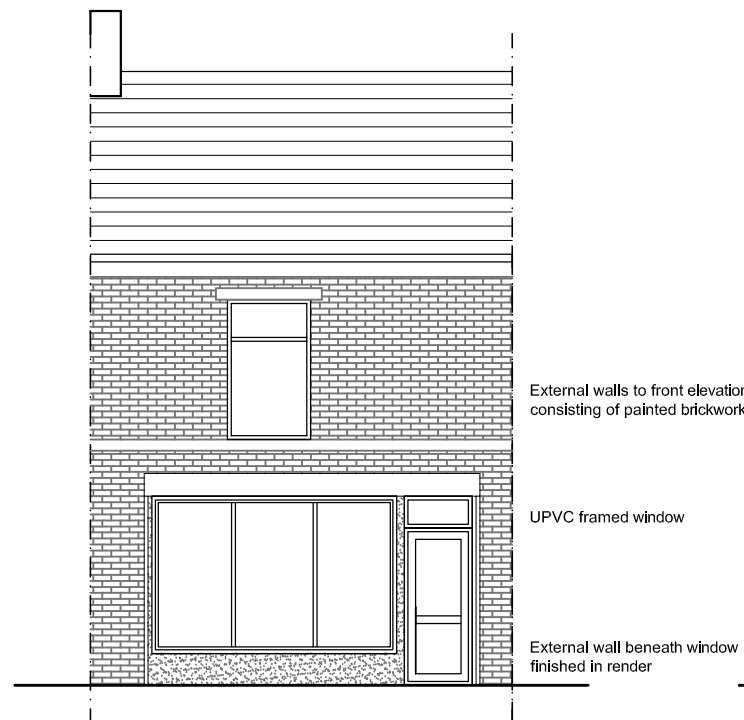




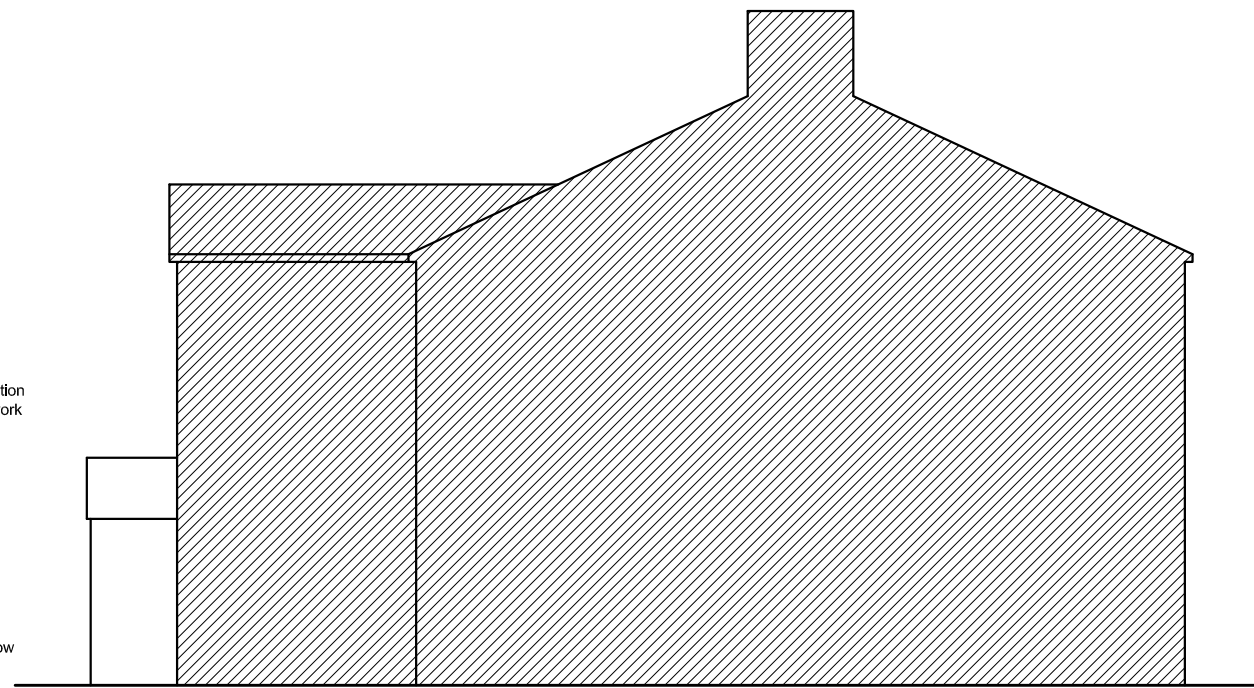
Existing Ground Floor Plan



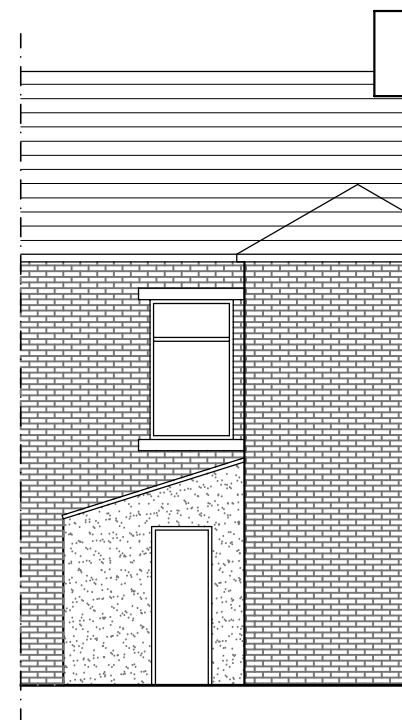
Existing First Floor Plan



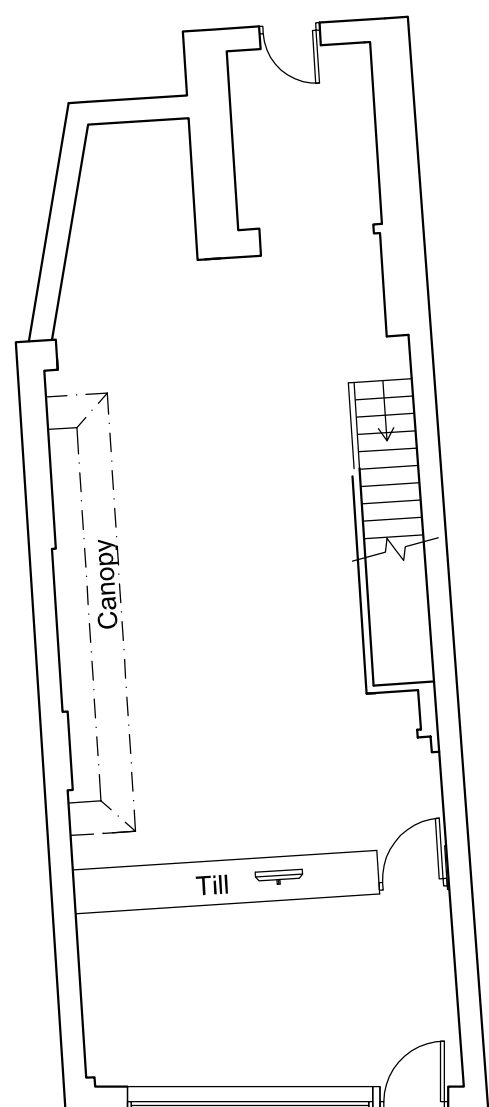
Existing Front Elevation



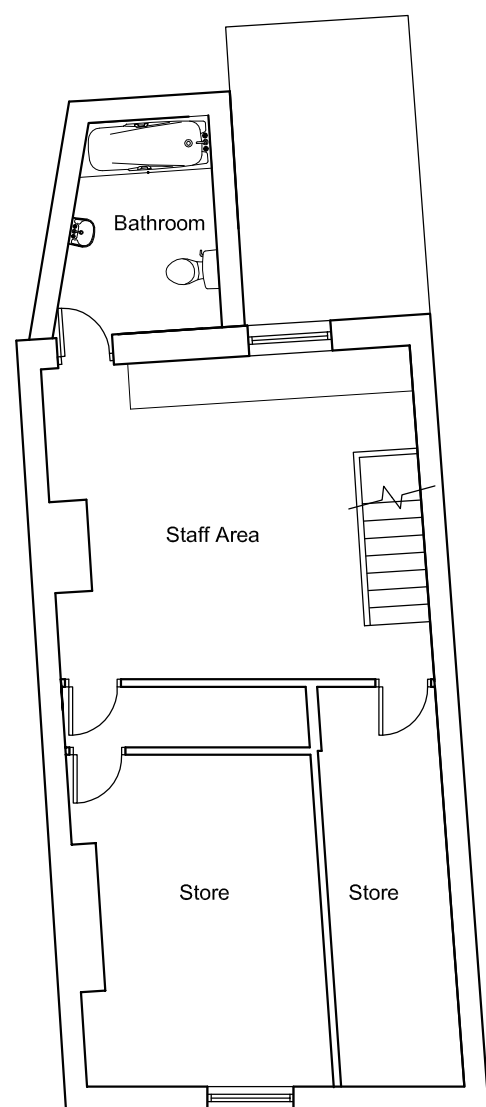
Existing Side Elevation



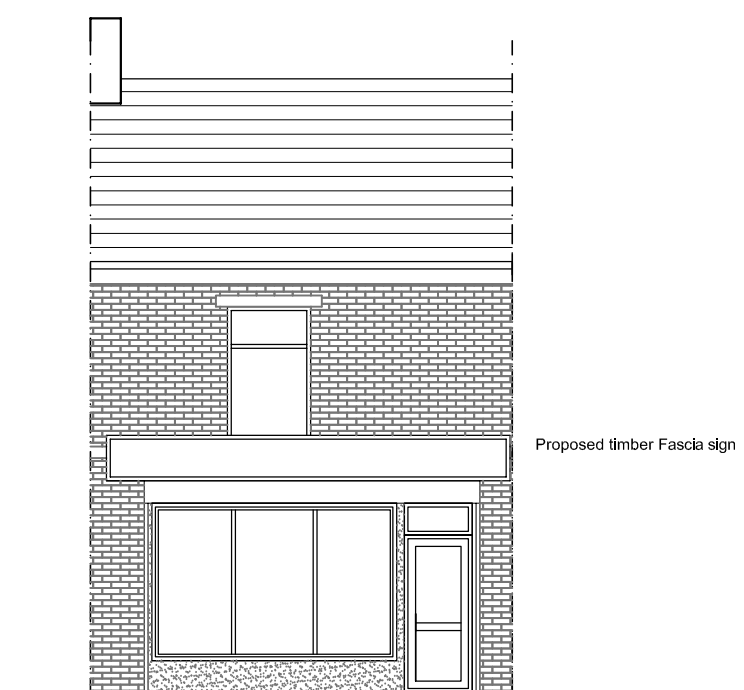
Existing Rear Elevation



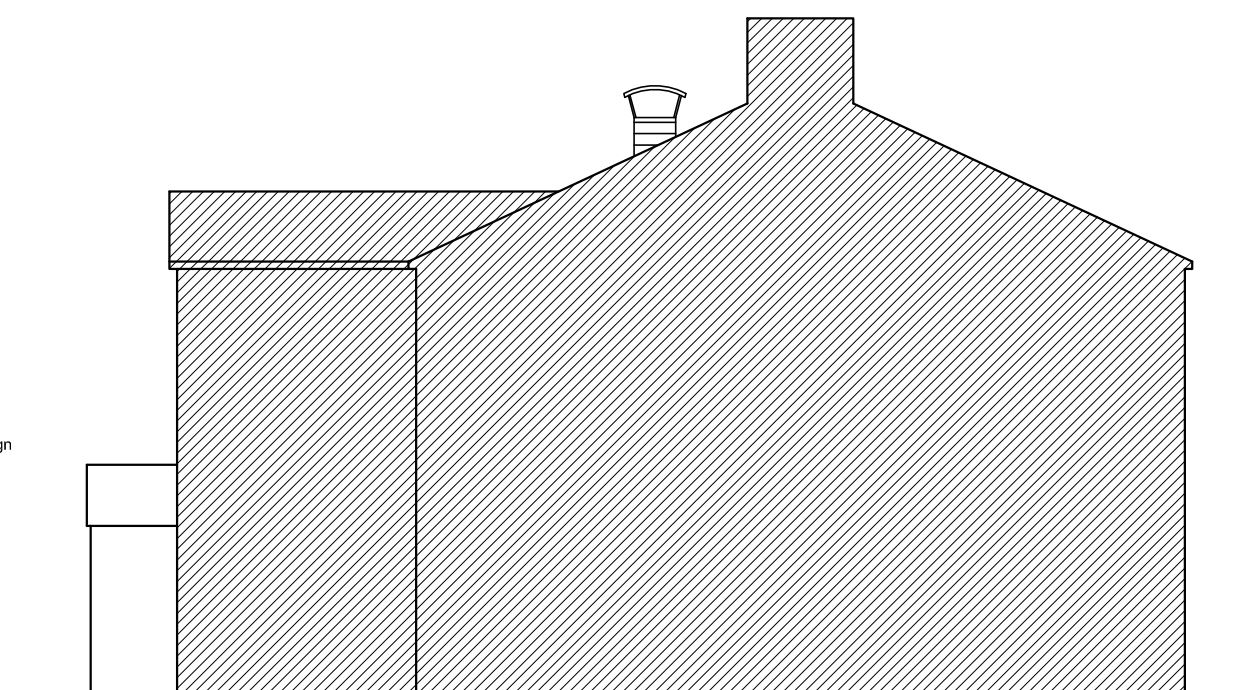
Proposed Ground Floor Plan



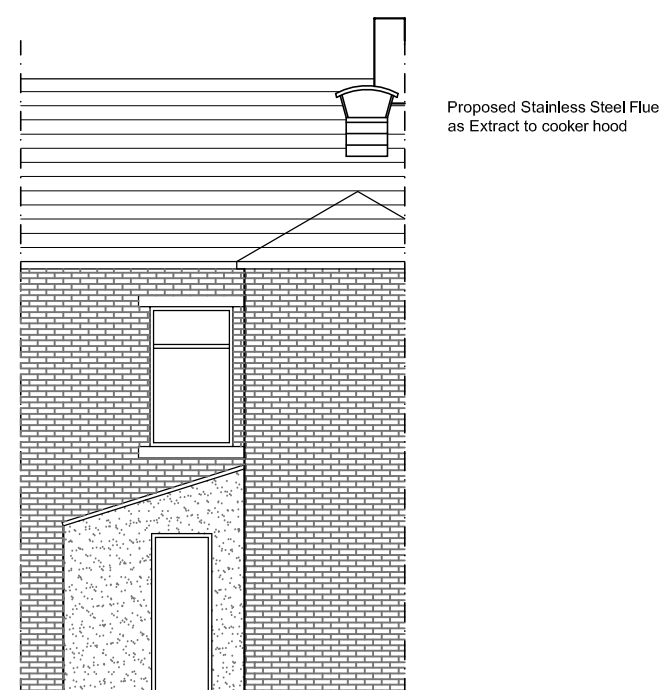
Proposed First Floor Plan



Proposed Front Elevation



Proposed Side Elevation



Proposed Rear Elevation

Notes:

All work is to be carried out to the latest current British standards Codes of Practice and recognised working practices.

All work and materials should comply with Health and Safety legislation and to be approved by the Local Authority Planning / Building Control Officer.

All dimensions are in millimetres unless where explicitly shown otherwise.

The contractor should check and clarify all dimensions as work proceeds and notify the design team of any discrepancies.

Do not scale off the drawings, if in doubt ask.

Avalon Chartered Town Planning are not liable for work undertaken prior to Full Planning Consent and/or Building Regulations Approval

Amendment A: 06.01.17 First Floor Plan added

Avalon
Chartered Town Planning

Town Planning - Architectural Design - Building Regulations - Surveying

Phone: 01282 834834 **Fax:** 01282 451666
Web: www.avalontp.co.uk **Email:** planning@avalontp.co.uk
2 Reedley Business Centre, Redman Road, Burnley, Lancashire, BB10 2TY

EXISTING AND PROPOSED PLANS AND ELEVATIONS						
Site: 29 Bolton Road Bury BL8 2AB						
Client: Safraz Ali						
Date: 22.08.16		Scale: 1:100 @ A1				
Project No: ARSH/02 Dwg 02A				Drawn: DS		
Amendments:	A					

Ward: Ramsbottom and Tottington -
Ramsbottom

Item 05

Applicant: Mr Robert Hodkinson

Location: Former Ramsbottom Police Station, Bridge Street, Ramsbottom, Bury, BL0 9AB

Proposal: Demolition of former police station and erection of building to include 2 no. shop units with 8 no. apartments above

Application Ref: 60708/Full

Target Date: 29/12/2016

Recommendation: Approve with Conditions

The applicant for this application is a Councillor.

A site visit has been requested by the Development Manager.

Description

The application site contains two single storey buildings comprising a former police station and garaging. Both are constructed from brick with a flat roof, one is located centrally and a detached garage is located in the northwest corner. Vehicular and pedestrian access is taken from Bridge Street with a second pedestrian access onto Garden Street. There is a brick wall with timber fencing above on the boundary to Garden Street.

The site is located within the Ramsbottom Conservation Area and is a town centre site. St Paul's church is located to the north west of the site and is a Grade II listed building with memorial gardens to the west. The Natwest Bank is located to the east and there is a post box adjacent to the gardens, which are both on the draft local list. There are apartments to the north and retail units with residential accommodation above to the south.

The proposed development involves the demolition of the existing police station and garage buildings and the erection of a three storey building. The proposed building would contain 2 retail units on the ground floor with 8 apartments above. The proposed building would be located on the Bridge Street frontage and would be constructed from stone with a slate roof. Pedestrian access would be achieved from Bridge Street. No parking is proposed and a yard would be located to the rear of the building. Servicing would take place on Bridge Street.

Relevant Planning History

37144 - resurfacing and associated works to car park - comprising erection of retaining walls and timber boarded fencing and lighting at Ramsbottom Police Station, Bridge Street, Ramsbottom. Approved with conditions - 1 December 2000.

01848/E - Proposed demolition of former police station and erection of 2 no. shop units with 8 no. apartments over at Former Police Station, Bridge Street, Ramsbottom. Enquiry completed - 9 August 2016.

Publicity

The neighbouring properties were notified by means of a letter on 4 November 2016 and a press notice was published in the Bury Times on 10 November 2016. Site notices were posted on 10 November 2016 on site.

2 letters have been received from the occupiers of 7, 11 Garden Street, which have raised the following issues:

- The building will be an eyesore and is much larger than the current building.

- The proposed building will block light and infringe on our privacy.
- Garden Street is already congested and is in a poor state of repair.
- Another application has been submitted on Garden Street (60681) and if these go ahead it will be like living in the middle of a building site.

The neighbouring properties and objectors were notified of the revised plans on 15 December 2016.

Two letters have been received from the occupiers of 14 Bridge Street and 22 Roseberry Close, which have raised the following issues:

- The proposal should not be allowed based upon the size of the building, which is far higher than the neighbouring bank.
- The proposed building would dominate the view of the church.
- Concerns about parking for residents and their visitors. Bridge Street is already too busy.
- The shop units should have restrictions placed on them as there are far too many takeaways in Ramsbottom.

The objectors have been notified of the Planning Control Committee meeting.

Consultations

Traffic Section - No objections, subject to the inclusion of conditions relating to access details and restricting access between the site and Garden Street.

Drainage Section - Comments awaited.

Environmental Health - Contaminated Land - No objections, subject to the inclusion of conditions relating to contaminated land.

Environment Health - Air Quality - No comments.

Environmental Health - Commercial Section - No response.

Environmental Health Pollution Control - No response.

Waste Management - Bin store will need to be large enough to accommodate 9 240ltr bins.

United Utilities - No objections, subject to the inclusion of conditions relating to foul and surface water drainage and SuDS.

GM Ecology Unit - No objections, subject to the inclusion of informatives relating to bats and nesting birds.

Unitary Development Plan and Policies

H1/2	Further Housing Development
H2/1	The Form of New Residential Development
H2/2	The Layout of New Residential Development
EN1/1	Visual Amenity
EN1/2	Townscape and Built Design
EN1/3	Landscaping Provision
EN1/8	Shop Fronts
EN2/1	Character of Conservation Areas
EN2/2	Conservation Area Control
EN2/3	Listed Buildings
EN5/1	New Development and Flood Risk
EN6/3	Features of Ecological Value
EN7	Pollution Control
EN7/2	Noise Pollution
EN7/5	Waste Water Management
EN8/2	Woodland and Tree Planting
RT2/2	Recreation Provision in New Housing Development
S1/2	Shopping in Other Town Centres
S2/1	All New Retail Proposals: Assessment Criteria
HT2/4	Car Parking and New Development
HT4	New Development
HT5/1	Access For Those with Special Needs
TC2/1	Upper Floors

Area	Bolton Street/Bridge Street
RM3	
SPD1	Open Space, Sport and Recreation Provision
SPD6	Supplementary Planning Document 6: Alterations & Extensions
SPD11	Parking Standards in Bury
NPPF	National Planning Policy Framework

Issues and Analysis

The following report includes analysis of the merits of the application against the relevant policies of both the National Planning Policy Framework (NPPF) and the adopted Bury Unitary Development Plan (UDP) together with other relevant material planning considerations. The policies of the UDP that have been used to assess this application are considered to be in accordance with the NPPF and as such are material planning considerations. For simplicity, just the UDP Policy will be referred to in the report, unless there is a particular matter to highlight arising from the NPPF where it would otherwise be specifically mentioned.

Principle (Residential) - Following revocation of the North West Regional Strategy on 20 May 2013, there is no statutory housing target for Bury. Work has commenced on the Greater Manchester Spatial Framework and this will bring forward a new statutory housing target for the Borough. This will subsequently be incorporated into Bury's future Local Plan.

In the meantime, the National Planning Policy Framework should be treated as a material planning consideration and it emphasises the need for local planning authorities to boost the supply of housing to meet local housing targets in both the short and long term. There is a particular emphasis, as in previous national planning guidance, to identify a rolling five year supply of deliverable housing land.

Policy H1/2 states that the Council will have regard to various factors when assessing a proposal for residential development, including whether the proposal is within the urban area, the availability of infrastructure and the suitability of the site, with regard to amenity, the nature of the local environment and the surrounding land uses.

The proposed development is located within the urban area and there are residential dwellings above the retail units in the locality. As such, the proposed development would not conflict with the surrounding land uses. The site is located in the town centre and has good access to public transport and as such, is in a sustainable location. The site contains the former police station and an area of hardstanding and is previously developed land. Therefore, the proposed development would be acceptable in principle and would be in accordance with Policy H1/2 of the Bury Unitary Development Plan and the NPPF.

Principle (Retail) - Policy S1/2 states that the Council will seek to maintain and strengthen the retail roles of Ramsbottom, Radcliffe and Prestwich town centres. The Council will support modest increases in shopping floorspace. However the main emphasis will be on consolidation and enhancement.

Policy S2/1 states that the Council will support new retail development proposals which accord satisfactorily with the following factors:

- are within or immediately adjoining the main shopping area of existing centres;
- sustain or enhance the vitality and viability of a centre;
- are accessible by public and private transport;
- are in conformity with other policies and proposals of the Plan.

Having successfully established the principle of development, all retail proposals will be considered with regard to their environmental impact and must take account of the following factors:

- they should have regard to their surroundings in terms of design, scale, height, bulk, use and colour of materials;
- the effects on the environment of the area as a result of increased traffic generation and servicing;

- the design of the proposal with regard to the safety and security for shoppers, workers, visitors and surrounding occupiers;
- the effects on the amenity of nearby residents or businesses by reason of noise, smell, litter or opening hours;
- access and facilities for the mobility impaired;
- the provision of adequate car parking and servicing.

Area RM3 states that the Council will encourage and support proposals for retailing and appropriate ancillary uses within the Bolton Street/Bridge Street area, the town's main shopping area.

The proposed development would provide 200 square metres of retail floorspace at ground floor level and would be located in the main shopping area of the town centre. The proposed development would be accessible by public and private transport and would enhance the viability and vitality of the centre. Therefore, the proposed development would be acceptable in principle and would be in accordance with Policies S1/2 and S2/1 of the Bury Unitary Development Plan.

Impact upon heritage assets - The national legislative framework for development affecting listed buildings and conservation areas is provided by the Planning (Listed Buildings and Conservation Areas) Act 1990. This sets out the duty on local planning authorities with regard to listed buildings and any buildings or land within a conservation area, when determining applications for planning permission. It is essential that these legal duties are considered, alongside the contents of the NPPF and other planning policies and guidance.

The Town and Country Planning (Listed Building and Conservation Area) Act 1990 imposes a duty under Section 72 on Local Planning Authorities to pay 'special attention to the desirability of preserving or enhancing the character or appearance of that area' (section 72(1))

Paragraph 131 of the NPPF states that when determining planning applications, Local Planning Authorities should take account of:

- the desirability of sustaining and enhancing the significance of heritage assets and putting them to viable uses consistent with their conservation;
- the positive contribution that conservation of heritage assets can make sustainable communities including their economic vitality;
- the desirability of new development making a positive contribution to local character and distinctiveness.

Paragraph 133 of the NPPF states that where a proposed development would lead to substantial harm or total loss of significance of a designated heritage asset, local planning authorities should refuse consent, unless it can be demonstrated that the substantial harm or loss is necessary to achieve substantial public benefits that outweigh that harm or loss, or all of the following apply:

- the nature of the heritage asset prevents all reasonable uses of the site; and
- no viable use of the heritage asset itself can be found in the medium term through appropriate marketing that will enable its conservation; and
- conservation by grant funding or some form of charitable or public ownership is demonstrably not possible; and
- the harm or loss is outweighed by the benefit of bringing the site back into use.

Policy 24 of the Conservation Area management plan identifies this site as an "issue to be addressed due to the design and appearance of the existing police station". As such, it is considered that the existing buildings do not contribute to the character of the conservation area in a positive way and their demolition would enhance the character of the conservation area and the setting of the Grade II listed building and the locally listed buildings.

The proposed building would be viewed from the west and south west, especially through

the gardens. The proposed building would be constructed from natural stone and slate and maintains/incorporates features of the local vernacular. The proposed development would impact upon the setting of the Grade II listed church. However, the proposed building would be some distance from the listed building and would reinforce to frame the listed building and its curtilage by protecting the gardens and the setting of the listed building, which the current buildings do not. The existing building contributes little to the setting of the listed building and given the distance between the proposed and listed building, the proposed development would preserve the setting of the listed building.

The design approach utilises the local form in its elevational composition and the use of headers, cills, banding, quoins and pike details would add visual interest to the building and would complement the existing buildings in the locality. The proposed building would be three storeys in height, which reflects the existing buildings on Bridge Street, where there is a mix of two and three storey buildings already in existence.

As such, the proposed development would preserve the setting of the nationally listed and locally listed buildings and would enhance the character of the Conservation Area. Therefore, the proposed development would be in accordance with Policies EN1/1, EN1/2, EN2, EN2/1, EN2/2 and EN2/3 of the Bury Unitary Development Plan and the NPPF.

Design and layout - The proposed building would be three storeys in height with a pitched roof and would be constructed from natural stone and slate, which would be appropriate. The use of headers, cills, banding, quoins and pike details would add visual interest to the building and the provision of a banding would tie in with the bank building immediately adjacent.

The proposed building would be a storey higher than the adjacent bank. However, as it is considered to be a framing building in terms of the listed building curtilage, buildings opposite and on the corresponding side of the gardens are also three storey. The bank adjacent is not a listed building and the lesser height corresponds with a reducing scale that progresses easterly. As such, the three storey height relationship to the bank is considered appropriate. Therefore, the proposed development would not be a prominent feature in the locality and would be in accordance with Policies S2/1, H2/1 and H2/2 of the Bury Unitary Development Plan.

Impact upon surrounding area - The proposed apartments would have access to private amenity space at the side and rear of the proposed building. A bin store would be provided to the rear of the bank building, which would be screened by timber fencing and landscaping, which would be acceptable. As such, the proposed development would not be a prominent feature and would be in accordance with Policies EN1/2, H2/1 and H2/2 of the Bury Unitary Development Plan.

Impact upon residential amenity - SPD6 provides guidance on aspect standards between residential properties and as such, would be relevant in this case. The aspect standards states that there should be a minimum of 20 metres between directly facing habitable windows and 13 metres between a habitable room window and a two storey blank wall. If there is an additional storey in height, a further 3 metres should be added to the aspect standard.

There would be 1.2 metres between the proposed building and the blank gable elevation to the Natwest Bank. This would be acceptable and would comply with the aspect standards.

There would be 23 metres to the rear elevation of the apartments on Church Street. This would comply with the aspect standard of 23 metres. There would be 11.8 metres between the proposed building and the blank gable of the apartments on Garden Street, which would be acceptable.

Therefore, the proposed development would not have an adverse impact upon the amenity of the neighbouring properties or the future occupiers of the proposed apartments.

Bats - A bat survey was submitted with the application, which states that both buildings have low bat roosting potential and no signs of roosting bats were found. The immediate area was considered to have low bat foraging potential with high foraging potential within 200 metres of the site. GM Ecology Unit has no objections to the proposed development, subject to the inclusion of informatives relating to bats and nesting birds. Therefore, the proposed development would not harm a protected species and would be in accordance with Policies EN6 and EN6/3 of the Bury Unitary Development Plan.

Highways issues - Originally, the proposed development was to be accessed from Garden Street. However, this access would not meet the required visibility splays due to the location of the bank building. As such, this access has been deleted from the plans and loading bays would be located on Bridge Street reflecting a similar situation to what is there now. Pedestrian access would be taken from Bridge Street. The Traffic Section has no objections, subject to the inclusion of conditions relating to access details and restricting access between the site and Garden Street. Therefore, the proposed development would not be detrimental to highway safety and would be in accordance with Policies H2/1, H2/2 and EN1/2 of the Bury Unitary Development Plan.

Parking - SPD11 states that the maximum parking standards for residential development is 1 space per 1 bed unit and 1.5 spaces per 2 bed unit. The maximum parking standards for retail is 1 space per 25 square metres. This equates to 15 parking spaces.

The proposed development would not provide any off-street parking. The site is highly sustainable with has good access to public transport and would be located within the town centre. As such, zero parking provision would be acceptable in this instance. Therefore, the proposed development would be in accordance with Policy HT2/4 of the Bury Unitary Development Plan and SPD11.

Response to objectors

- The proposed building would be larger than the one it replaces, but it would be appropriate in terms of height and scale to the existing buildings in the locality.
- The issues relating to privacy, loss of light and traffic have been addressed in the main report.
- The impact of the development during construction is not a material planning consideration and cannot be taken into account.

Statement in accordance with Article 35(2) Town and Country Planning (Development Management Procedure) (England) (Amendment) Order 2015

The Local Planning Authority worked positively and proactively with the applicant to identify various solutions during pre-application discussions to ensure that the proposal comprised sustainable development and would improve the economic, social and environmental conditions of the area and would accord with the development plan. These were incorporated into the scheme and/or have been secured by planning condition. The Local Planning Authority has therefore implemented the requirement in Paragraphs 186-187 of the National Planning Policy Framework.

Recommendation: Approve with Conditions

Conditions/ Reasons

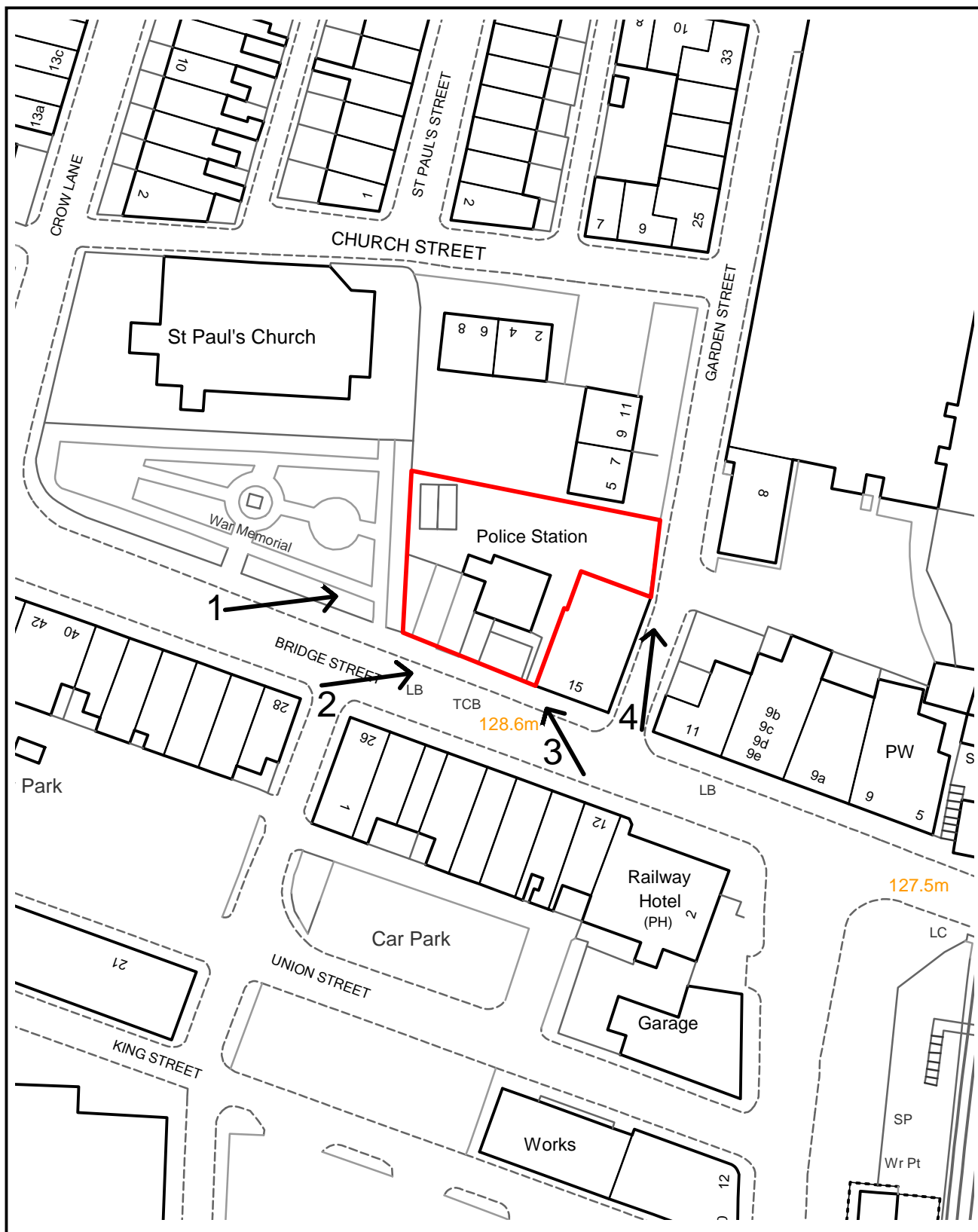
1. The development must be begun not later than three years beginning with the date of this permission.
Reason. Required to be imposed by Section 91 Town & Country Planning Act 1990.
2. This decision relates to drawings numbered TRI-1686-01, 16-102-20, 16-102-21, 16-102-26A, 16-103-22, 16-103-23, 16-103-24, 16-103-25, 16-103-26 and the development shall not be carried out except in accordance with the drawings hereby approved.
Reason. For the avoidance of doubt and to ensure a satisfactory standard of design pursuant to the policies of the Bury Unitary Development Plan listed.
3. No development shall commence unless and until:-
 - A contaminated land Preliminary Risk Assessment report to assess the actual/potential contamination and/or ground gas/landfill gas risks at the site shall be submitted to, and approved in writing by, the Local Planning Authority;
 - Where actual/potential contamination and/or ground gas/landfill gas risks have been identified, detailed site investigation and suitable risk assessment shall be submitted to, and approved in writing by the Local Planning Authority;
 - Where remediation/protection measures is/are required, a detailed Remediation Strategy shall be submitted to, and approved in writing by, the Local Planning Authority.Reason. The scheme does not provide full details of the actual contamination and subsequent remediation, which is required to secure the satisfactory development of the site in terms of human health, controlled waters, ground gas and the wider environment and pursuant to National Planning Policy Framework Section 11 - Conserving and enhancing the natural environment.
4. Following the provisions of Condition 3 of this planning permission, where remediation is required, the approved Remediation Strategy must be carried out to the satisfaction of the Local Planning Authority within agreed timescales; and A Site Verification Report detailing the actions taken and conclusions at each stage of the remediation works, including substantiating evidence, shall be submitted to and approved in writing by the Local Planning Authority prior to the development being brought into use.
Reason. To secure the satisfactory development of the site in terms of human health, controlled waters and the wider environment and pursuant to National Planning Policy Framework Section 11 - Conserving and enhancing the natural environment.
5. Details/Samples of all materials to be used in the external elevations, together with details of their manufacturer, type/colour and size, shall be submitted to and approved in writing by the Local Planning Authority before the development is commenced. Only the approved materials shall be used for the construction of the development.
Reason. No material samples have been submitted and are required in the interests of visual amenity and to ensure a satisfactory development pursuant to UDP Policy EN1/1 Visual Amenity.
6. No development shall commence unless and until details of surface water drainage proposals have been submitted to and approved by the Local Planning Authority. The proposed surface water scheme must be based on the hierarchy of drainage options in the National Planning Practice Guidance. It must be designed in accordance with the Non-Statutory Technical Standards for Sustainable Drainage Systems (March 2015). This must include assessment of potential SuDS options for surface water drainage with appropriate calculations and test results to support the chosen solution. Details of proposed maintenance arrangements for any SuDS features should also be provided.
Reason. The current application contains insufficient information regarding the

proposed drainage scheme to fully assess the impact. The applicant is required to demonstrate that the development would not increase flood risk for residents or adjacent properties. To promote sustainable development and reduce flood risk pursuant to Bury Unitary Development Plan Policy EN5/1 - New development and Flood Risk and chapter 10 - Meeting the challenge of climate change, flooding and coastal change of the NFFP.

7. Foul and surface water shall be drained on separate systems.
Reason. To ensure a satisfactory means of drainage, promote sustainable development and to manage the risk of flooding and pollution pursuant to Policy EN5/1 - New Development and Flood Risk and Policy EN7/5 - Waste Water Management of the Bury Unitary Development Plan and Section 10 of the National Planning Policy Framework.
8. Notwithstanding the details indicated on approved plan reference 16-102-26 Revision A and 16-103-22, no development shall commence unless and until full details of the following have been submitted to and approved in writing by the Local Planning Authority:
 - The reinstatement of the redundant footway crossing along the Bridge Street site frontage, lifting of the footway to meet existing back of footway levels at the westerly and easterly site boundaries and provide adequate falls to the carriageway for the purpose of discharging surface water, proposed level thresholds to the shop units and apartment entrance at this increased height above existing levels and all associated highway remedial works;
 - Foundation and surface water drainage details for the proposed development demonstrating that no encroachment of or discharge onto the existing adopted highway will occur;
 - A review of the existing traffic regulation orders on Bridge Street in the vicinity of the site to revoke the existing prohibition of loading, formalisation of a bus stop clearway and introduction of peak hours loading restrictions, the extents of which to be agreed, including all associated provision/alteration of road markings, signage and street furniture;
 - Submission of a method statement to cover the delivery and storage of materials and all works abutting the adopted highway, including the provision, where necessary of temporary pedestrian facilities/protection measures on Bridge Street.The details subsequently approved shall be implemented to an approved specification and programme.
Reason. To ensure good highway design and maintain the integrity of the adopted highway in the interests of highway safety and to provide adequate loading facilities on the adopted highway for the benefit of the future development pursuant to the following Policies of the Bury Unitary Development Plan:
Policy H2/1 - The Form of New Residential Development
Policy H2/2 - The Layout of New Residential Development
Policy EN1/2 - Townscape and Built Design.
9. There shall be no direct means of temporary or permanent vehicular access between the site and Garden Street.
Reason. To ensure good highway design in the interests of road safety pursuant to the following Policies of the Bury Unitary Development Plan:
Policy H2/1 - The Form of New Residential Development
Policy H2/2 - The Layout of New Residential Development
Policy EN1/2 - Townscape and Built Design.

For further information on the application please contact **Helen Longworth** on **0161 253 5322**

Viewpoints



PLANNING APPLICATION LOCATION PLAN

APP. NO 60708

**ADDRESS: Former Ramsbottom Police Station
Bridge Street, Ramsbottom**

Planning, Environmental and Regulatory Services

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Bury
COUNCIL

60708

Photo 1



Photo 2



60708

Photo 3



Photo 4



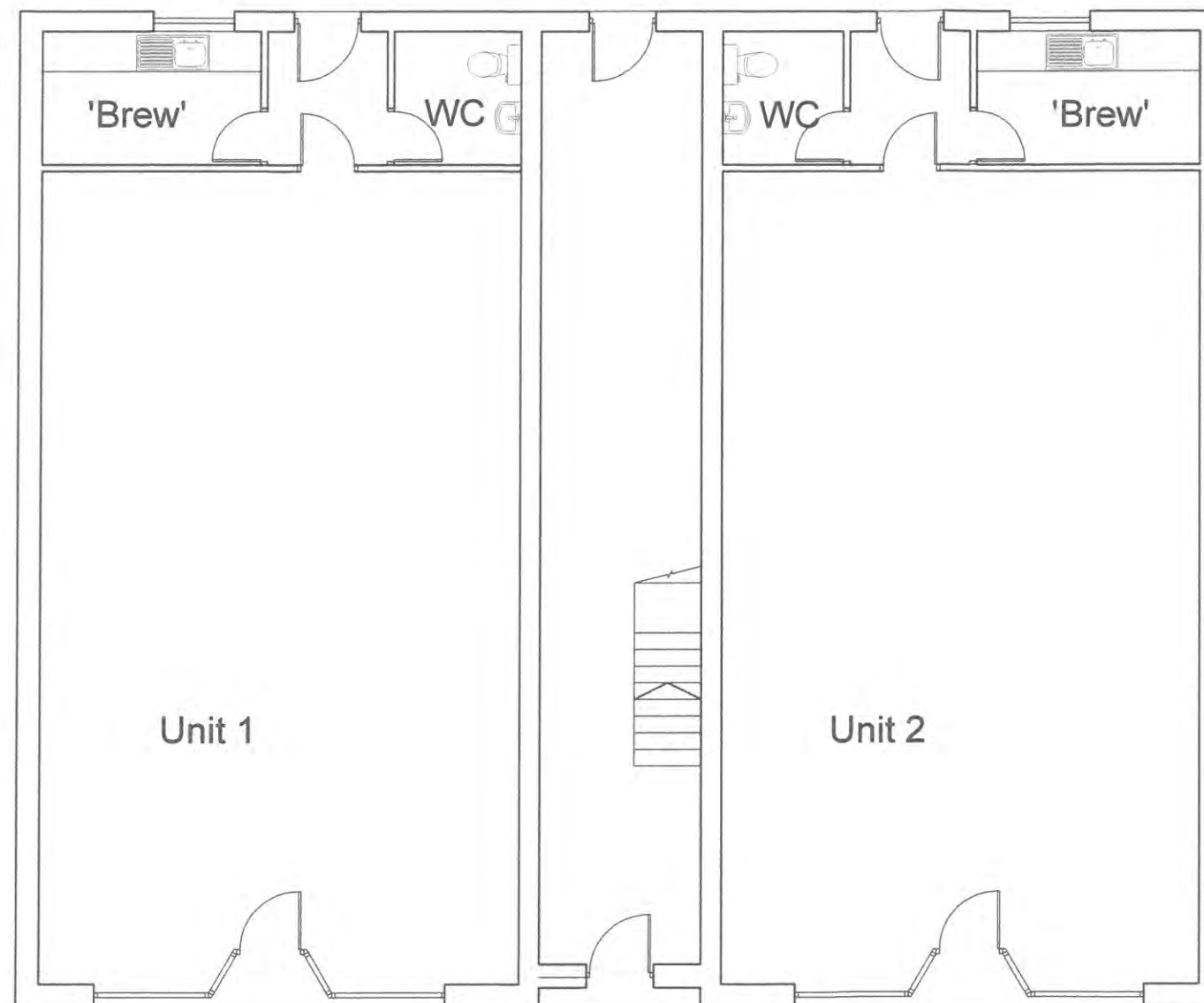
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all works to be carried out in accordance with current health and safety regulations.

do not scale from prints.

contractor to check all dimensions on site prior to commencement of works.



client: Mr and Mrs R Hodkinson

project: Proposed Development, Bridge Street,
Ramsbottom

drawing: Ground Floor Layout

scale: 1/100

date: October 2016

drawing no.: 16-102-20

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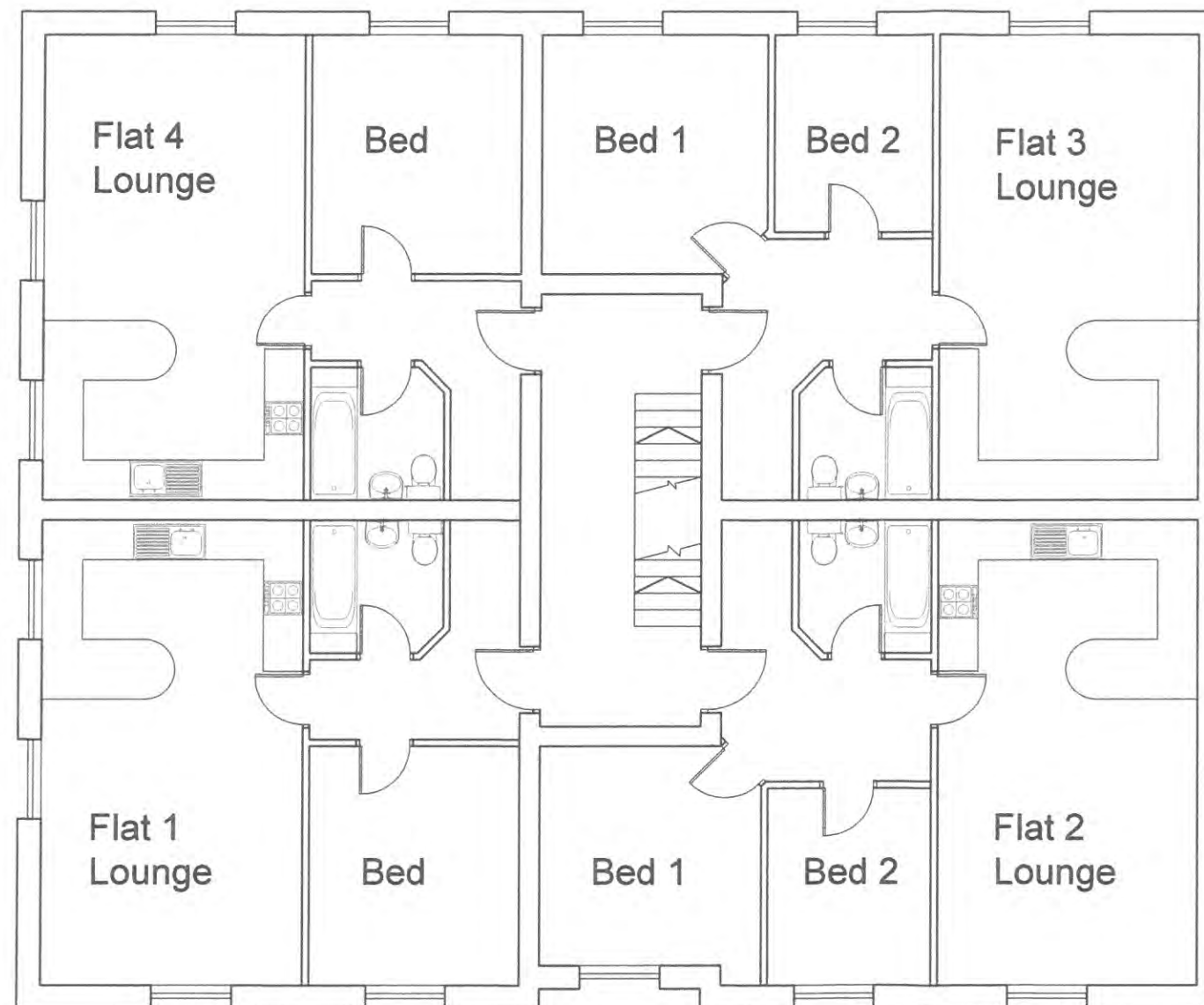
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2 no single bedroom units and 2 no two bedroom units
at first and second floors

client: Mr and Mrs R Hodkinson

project: Proposed Development, Bridge Street,
Ramsbottom

drawing: First Floor Layout

scale: 1/100

date: October 2016

drawing no.: 16-102-21

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Front Elevation to Bridge Street

client: Mr & Mrs R Hodkinson

project: Proposed Development at Bridge Street,
Ramsbottom

drawing: Proposed Front Elevation

scale: 1/100

date: October 2016

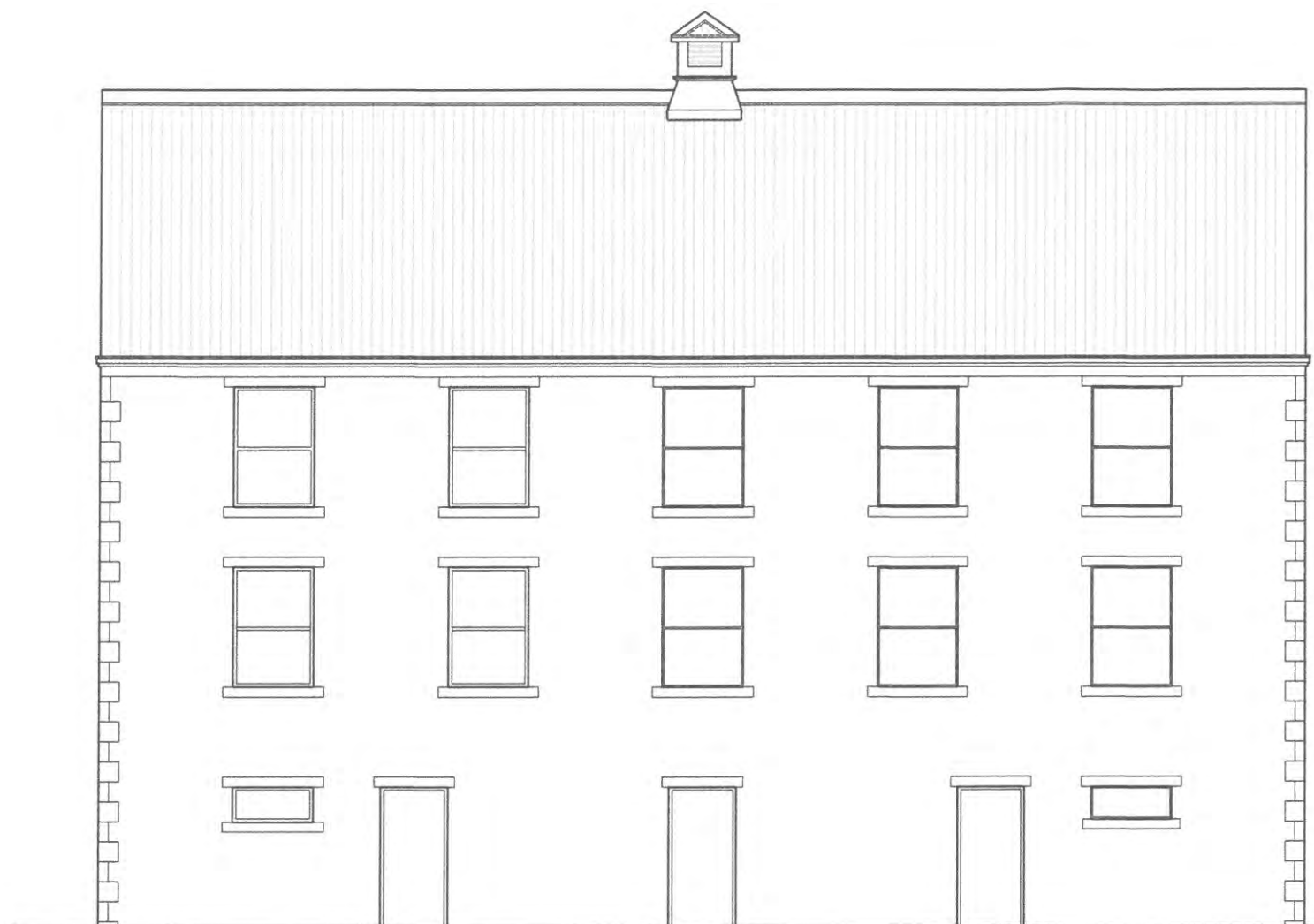
drawing no.: 16-103-22

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Rear Elevation

Front elevation and the Western gable, to the churchyard, to be in coursed natural stone, with stone heads, cills and jambs. Rear and eastern gable to be render finished, colour to be agreed. Stone heads and cills to the rear windows and doors, with stone quoins as indicated

Hardwood double glazed windows.

Ogee aluminium gutters with square section downpipes

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contractor to check all dimensions on site prior to commencement of works.

client: Mr & Mrs R Hodkinson

project:

drawing: Proposed Rear Elevation

scale: 1/100

date: October 2016

drawing no.: 16-103-24

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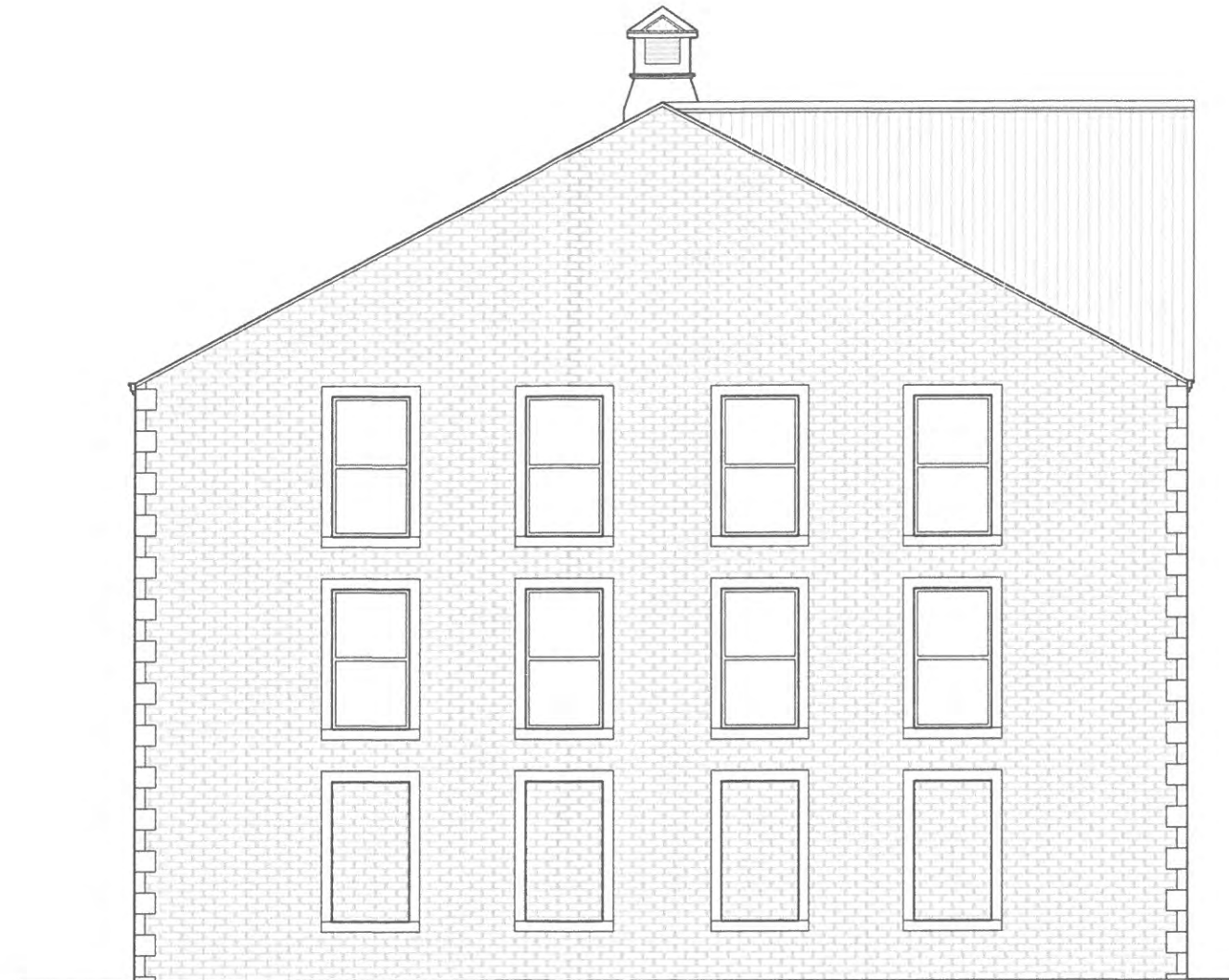
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do not scale from prints.

contractor to check all dimensions on site prior to commencement of works.



Gable to St Paul's Church

client: Mr & Mrs R Hodkinson

project: Proposed Development at Bridge Street,
Ramsbottom

drawing: Proposed GableElevation

scale: 1/100

date: October 2016

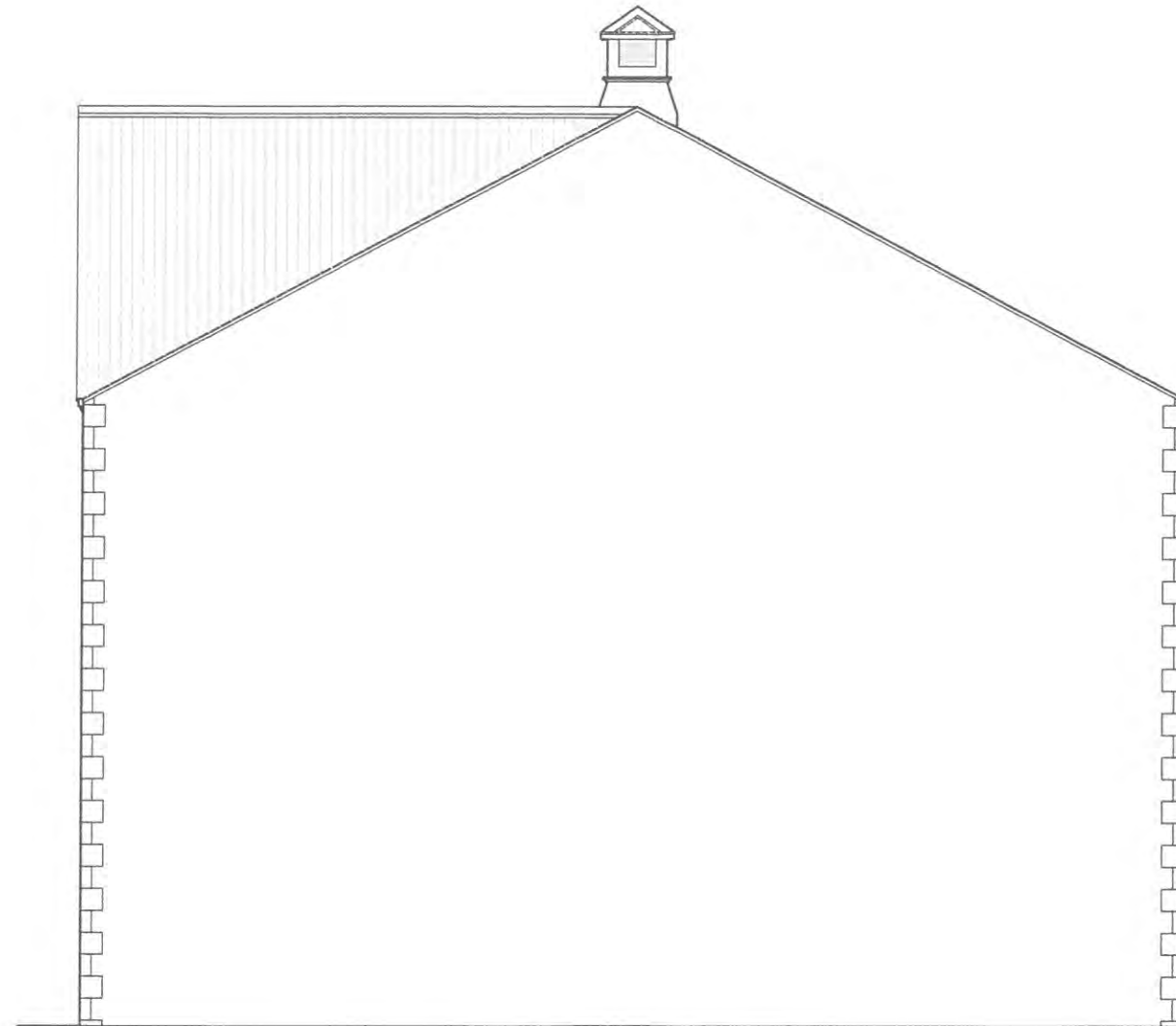
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all works to be carried out in accordance with current health and safety regulations.

do not scale from prints.

contractor to check all dimensions on site prior to commencement of works.

Gable to the Bank

client: Mr & Mrs R Hodkinson

project: Proposed Development at Bridge Street,
Ramsbottom

drawing: Proposed GableElevation

scale: 1/100

date: October 2016

drawing no.: 16-103-25

abacus architectural limited

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Ward: Bury East - Redvales

Item 06

Applicant: Volvo Hatfields of Bury

Location: Hatfields, Manchester Road, Bury, BL9 9XX

Proposal: Demolition of showroom section of existing facility and erection of new showroom, refurbishment and extension of workshop to include new MOT and valet bays and amendments to site parking and display layouts.

Application Ref: 60732/Full

Target Date: 16/02/2017

Recommendation: Approve with Conditions

Description

The application relates to a site of 5930 sqm which accommodates a car showroom business. The site fronts onto Manchester Road and is separated from the pedestrian footpath by a wide grass verge. It is bounded to the south by the River Roch and residential properties to the east on Lawrence Street. To the north, directly adjacent is a restaurant, beyond which is a garage dealership and builders merchants separated from the site by Lawrence Street.

The showroom has a footprint of 1305 sqm (equates to 22% of the site area) and fronts onto part of Manchester Road. The ground floor accommodates the showroom and customer service areas at the front with office space, workshop, MOT and servicing area to the rear of the building. The showroom has been extended single storey on the southern elevation following the grant of permission in 2012, which is also used as a car display area. A mezzanine covers approximately half the area of the ground floor and is used as offices and storage area.

The external facade comprises a mix of brick, glazing and cladded panels with corrugated roofing sheets and rooflights.

The remainder of the remainder of the site comprises forecourt display areas, customer and staff parking and stock and storage areas. Access for all vehicles including deliveries, is directly off Lawrence Street to the north which leads into the site through a set of double gates.

The application seeks the demolition of part of the showroom and its redevelopment to extend the building with a new build showroom, refurbishment of and extension to the workshop to include new MOT and valet bays and the re-configuration of the display layouts, staff and customer parking areas, within the remainder of the site.

The development would result in a built footprint of 1580 sqm, 26.7% of the overall site area, and would comprise the following:

- New main showroom - 600 sq m
- Retained and refurbished side showroom display and office - 240 sqm
- Retained and refurbished workshop and servicing area - 625 sqm.
- New MOT bay and valeting - 115 sqm.
- A first floor of 400 sqm would provide offices and staff facilities.
- The height of the building would be reduced from 9.9m to 8.05m.

The external facade would be an aluminium framed glazed showroom with opaque vinyl film applied internally, with the customer entrance located on the north west elevation. Rendered panels and cladding would provide a contrast to the external appearance of the

building. The roof would be insulated composite panels with the existing workshop roof retained and made good. Hard standing areas would be tarmaced and parking and vehicle display areas demarcated in alternative brick paving.

Planting and soft landscaping would be enhanced along the frontage of Manchester Road and new planter beds incorporated to the forecourt display zone with a refuse bin store area located at the back of the site.

There are currently 42 staff (35 full time and 7 part time) which would increase to 50 in total (42 full time and 8 part time). The applicant states that not all part time staff are based at the site, and operate from other dealerships.

In terms of parking provision, there is currently 25 staff spaces and 11 customer spaces. It is proposed to increase this on site provision to 30 dedicated staff spaces and 17 customer spaces which would be separately allocated within the site, the customer spaces being located adjacent to the building and staff spaces along the eastern boundary of the site.

Access into the site for all vehicles including the car transporters, would be as existing from Lawrence Street. Deliveries to the site would be 1-2 times a week, carried out during the working hours of 8am to 5pm.

Relevant Planning History

59542 - Infill extension to create additional car show room space and office at first floor - Approve with Conditions 29/01/2016

Publicity

Site notice posted 15/12/2016

Press advert in the Bury Times 24/11/2016

59 letters sent on 17/11/2016 to properties at Nos 2-18 (evens) and 21-33 (odds) Lawrence Street; 652 - 690 (evens) and 701, 703, Flats 1-6, 731 Manchester Road, 1 Hollins Brow; Travis Perkins Chelsea Street; Hardys Gate Dumers Lane; 2 Leyland Street; 1,2,3,4,6,8,10,12,14 Leyton Drive.

Objections received from No 2,10, 31 Lawrence Street summarised as follows:

- Discrepancies in the Transport Statement and which fails to recognise the presence of parked vehicles along the length of Lawrence Street between Manchester Road and Chelsea Street that would restrict transporter access during business hours (the sweep plan shows the transporter driving over parked cars).
- The limited employee parking provision (14 spaces for 50+ staff) would add to the existing problems currently experienced by residents with the employees of Richard Cort and customers of Travis Perkins.
- The near misses arising from the road layout during rush hour especially when the view is blocked by transporters delivering to Richard Cort are not included.
- The noise impact indicates a scheme is not yet developed meaning no recommendations of equipment or control measures can be concluded.
- Temporary measures during construction e.g. hours/days/access by contractors are not defined.
- Document 00175485 refers to Bath Road indicating a copy and paste document possibly omitting aspects and impacts pertinent to Manchester Road application;
- Why does the access have to be via a residential street when other garages have access via Manchester Road;
- The space between Selekt display and customer parking is wider than Lawrence Street - why can't this be used for access;
- There is already disturbance/congestion from Hatfield's, Richard Corts, Travis Perkins - all having deliveries/customers throughout the day as well as employees parking on Lawrence Street - are 14 staff bays sufficient?
- Increase in fumes and pollution;
- Is there a need to change access to Hatfield's from Manchester Road, or for an

Automatic Urban and Rural Monitoring Network monitor to be installed on Lawrence Street?

- Existing light pollution from the intense lights (security?) at the rear of Hatfield's - expansion will mean more lights?;
- The proposed changes would deny access to the gate at the side of No 2, needed for a number of reasons including access to bins.
- The expansion would lead to more noise, congestion and parking concerns for residents on Lawrence Street.

The applicant has responded to the objections as follows:

- Display space removed to allow the transporter to turn without having to clear the space for cars;
- Revised plan to show 30 staff spaces. The operator has advised that from the 40 staff employed, there is a demand for 25 parking spaces;
- Customer parking has been increased from 14 to 17 spaces;
- Swing gates inside the site would allow a transporter to manoeuvre on site and maintain security;
- Some of the concerns raised relate to the general situation of the vicinity and not specifically to this proposal. Hatfield cannot enforce parking allowances on Lawrence Street, and as it is the only access to the site, all efforts are made to keep this access clear as much as possible;
- transported deliveries to the site are only once a week, usually out of working hours;
- The other businesses in the area have been identified by the objectors as being culpable for access and parking issues on Lawrence Street;
- The mechanical plant scheme would be developed to ensure the scheme is viable and fit for purposes required;
- Arrangements of construction and duration of works would be covered in the Construction Phase Plan and Code of Construction to be developed to minimise disruption to both the business and local community.

Revised proposed site plan received 5/1/2017 to show increase in on-site staff parking provision for 30 staff spaces and 17 customer spaces and the removal of a car in the 'selekt' display area to enable manoeuvrability within the site.

Properties at Nos 2-10 (evens) and 21, 23 and 31 Lawrence Street notified of the revised plan.

The objectors have been notified of the Planning Control Committee meeting.

Consultations

Traffic Section - No objection subject to conditions.

Drainage Section - No response received. A condition to submit a drainage strategy is recommended.

Environmental Health Contaminated Land - No objection subject to conditions.

Environmental Health Pollution Control - No response received.

Environment Agency - No response received. The site does not fall within a flood zone and the submitted Flood Risk Assessment concludes that the site would not be at a significant risk from flooding.

Greater Manchester Police - designforsecurity - No objection. Recommend security measures be incorporated into the design of the development.

Greater Manchester Ecology Unit - No objection subject to conditions and informatives.

United Utilities - No objection in principle subject to a condition that the development be carried out in accordance with the submitted flood risk assessment.

Unitary Development Plan and Policies

NPPF	National Planning Policy Framework
EC6/1	New Business, Industrial and Commercial
EN1/2	Townscape and Built Design

EN7	Pollution Control
S4/4	Car Showrooms, Car Sales Areas and Petrol Filling Stns
HT2/4	Car Parking and New Development
HT6/2	Pedestrian/Vehicular Conflict
SPD11	Parking Standards in Bury

Issues and Analysis

The following report includes analysis of the merits of the application against the relevant policies of both the National Planning Policy Framework (NPPF) and the adopted Bury Unitary Development Plan (UDP) together with other relevant material planning considerations. The policies of the UDP that have been used to assess this application are considered to be in accordance with the NPPF and as such are material planning considerations. For simplicity, just the UDP Policy will be referred to in the report, unless there is a particular matter to highlight arising from the NPPF where it would otherwise be specifically mentioned.

Principle - Central to the principles of the NPPF is the presumption of sustainable development which advocates that the need for planning to play a positive economic, social and environmental role. Chapter 20 states that Local Planning Authorities should plan proactively to meet the development needs of businesses and support the economy, with significant weight placed on the need to support economic growth through the planning system.

UDP Policy S4/4 - Car Showrooms, Car Sales Areas and Petrol Filling Stations states that they should be located within adjoining established shopping areas, along classified roads or within other industrial and commercial areas within the Borough. Factors to consider are:

- potential disruption to the free flow of traffic;
- provision of adequate, well laid out car parking and display facilities together with safe and convenient arrangements for access, circulation and egress;
- road safety and the safety of pedestrians;
- the amenity of nearby residents and businesses.

UDP Policy EC6/1 - Assessing New business, Industrial and Commercial development considers factors relating to:

- scale, size, density, layout, height and materials;
- access and car parking provision;
- landscaping and boundary treatment;
- effect on neighbouring properties;
- safety of employees, visitors and adjacent occupiers.

The site is an existing car showroom business, located on a main classified road and key route network through the Borough. The site is self contained, with all facilities and servicing to be provided within the existing boundary with access remaining as existing for staff, deliveries and customers. The application seeks improvements to the external and internal arrangements of the business to improve operational efficiency and performance. It is considered that the scale of the development would not result in over development of the site or cause harm to the residential or visual amenity of the area.

As such, the proposed development is considered to be acceptable in principle and would comply with UDP Policies S4/4, EC6/1 and the NPPF.

Layout - The proposed layout of the site would essentially remain the same as the existing, apart from a relatively minor extension to the front and side of the building and internal reconfiguration of the car display areas and customer and staff parking.

The front section of the showroom, (440 sqm) which faces Manchester Road would be demolished and replaced by a 2 storey flat roof extension totalling 600 sqm, which would be used as the new showroom and sales area. The rear part of the building would be retained

and this space reconfigured as the workshop and servicing areas. A smaller single storey extension with a footprint of 115 sqm would be added to the south elevation of the building which would be used as an MOT bay and 2 valet bays.

Externally, car sales display areas would be sited in the western and southern corners of the site and two areas to the west of the building, for a total of 45 cars. 17 customer parking bays would be centrally located within the site. Staff parking spaces for 30 cars (20 in a tandem arrangement at the rear, and 10 spaces along the boundary with No 2 Lawrence Street) would be provided. A stock and storage area would also be located at the rear of the site.

The front of the site would remain open, with the grass verge retained between the building and Manchester Road and existing boundary treatment would be retained and replaced where necessary.

Access from Lawrence Street would be maintained, and would facilitate all vehicular movements into the site including deliveries.

The layout of the proposed development would not fundamentally alter from the existing arrangement, and given there would be no increase in the area of the site, and that all operations associated with the development would be contained within the site and accommodated without compromise to highway safety or amenities of the area, the proposal is considered to be acceptable and would comply with UDP Policies S4/4 and EC6/1.

Design and appearance - The proposed extension would result in an overall reduction in height of the building by 1.85m and would incorporate a flat roof design. The building would have a modern and contemporary appearance through the use of glazed and cladded facades and white rendered panels.

A glazed customer entrance set within silver cladded panels would define the access into the building on the south east elevation, with black paving to visibly demarcate the pedestrian entrance.

The car display areas would be clearly zoned within the site, as would customer and staff parking allocations, by the use of different blocked paving and signage.

The design and appearance of the building would be appropriate for the modernisation and upgrade for the use of the site as a well established car showroom.

As such, the development is considered to be acceptable and would comply with UDP Policies EC 6/1 and EN1/2.

Impact upon residential amenity - The building would be extended across the front of the site and would be no closer to residential properties on Manchester Road than the existing built form. The single storey side extension would also not project past the existing footprint of the building and there would be no impact from this part of the development. Separation distances to all surrounding properties would not be compromised and would be acceptable.

The use of the site would continue to function and offer the same sales, servicing, valeting and MOT services as currently, the main difference being the location of these services within the site, which has been designed to improve the layout, circulation and shopping experience for the customer.

Whilst concerns have been raised by some of the residents with regards to parking provision within the site, this would be improved by the new layout. Ten of the staff spaces are proposed directly adjacent to No 2 Lawrence street. However, this area is already used as a used car display area and there would be no change to how this area of the site would appear. The use of this area for parking would also not increase activity or noise

than already exists. Access to the side of this property would be unaffected by the proposal, and residents of this property would have access to both their property and garden, as is currently the case.

The site operates daytime hours during the week and at weekends and there are no proposals to amend the current hours of operation.

Deliveries once or twice a week would continue to be carried out via the existing access from Lawrence Street and given the scale of the development, there would not be significant increases in traffic or footfall to the area than already exists.

As such, it is considered that the proposed development would not have a detrimental impact on residential amenity and would comply with UDP Policies S4/4, EC6/1 and EN1/2.

Highways issues

Access - Access into the site is off Lawrence Street to the north for all vehicles, including delivery vehicles, and this would be retained. A swept path analysis submitted on the layout plan demonstrates that these vehicles would be able to safely enter and manoeuvre within the site and exit onto Lawrence Street in a forward gear.

The scale of the development to increase the footprint of the showroom and reconfigure site arrangements would not result in a significant expansion of the business to cause any highways issues. The site is restricted within the existing boundary and the proposed layout plan demonstrates that all services, facilities and associated business requirements could be accommodated within the site without causing highway safety issues.

Parking - There are no parking standards in SPD11 with regard to car showroom uses.

The proposed development would provide 30 dedicated staff spaces in total, an increase of 5 spaces on the existing provision. These would be located to the rear of the site in a tandem arrangement.

17 customer spaces would be provided, and these would be located at the side of the showroom building in front of the customer entrance.

Objections have been raised by local residents regarding access, traffic and parking on Lawrence Street. Lawrence Street serves as an access for businesses, including a restaurant, builders merchants and the development site, as well as residential properties further up the street. A short distance of the lower end of Lawrence Street has parking restrictions, with the majority of the road unrestricted.

Cars already park on Lawrence Street, and this would not change as a result of the development. Lawrence Street is wide enough to cater for large delivery vehicles to the site, and whilst there may be cars parked on the road either legally or illegally is not a matter for the applicant to address under this proposal.

The Highway's Section has raised no objection to the proposals subject to the inclusion of conditions.

As such, it is considered that the proposed development would not be detrimental to highway safety and would provide acceptable levels of parking provision within the site for staff and customers, and would therefore be in compliance with UDP Policies S4/4, EN1/2 and HT2/4.

Noise - The existing use of the site includes car sales, servicing, repairs and valeting. The proposed development would incorporate these services internally within the building and any impact from noise emissions from the building would not be significantly different to the existing situation. In terms of the external mechanical plant equipment and condenser unit, these would be located directly next to the building and would be 22m from the garden of

the nearest residential property, No 6 Lawrence Street, and 50m from the rear elevation of No 2 Lawrence Street.

The noise assessment which has been submitted with the application has carried out a series of noise surveys to assess the impact of noise from the site. It was concluded that the impact of noise from the development on nearby residents would be low and in accordance and within the parameters of current guidance and standards of BS 4142:2014.

A condition that details of the noise output from plant equipment would be included to ensure the type and output would not affect local residents.

Ecology - A bat survey (Preliminary Roost Assessment), has been submitted with the application and GMEU have been consulted. GMEU are satisfied that adequate ecological information has been submitted and no further information would be required prior to determination. Any ecological constraints can be resolved by condition and/or informative.

Bats - The building to be extended has been assessed as having negligible bat roosting potential. Despite the high risk location, the finding in the report indicate the building is low risk and this is accepted by GMEU. An informative would therefore be acceptable.

Breeding birds - It would appear from the existing and proposed site layout that no trees would be lost. Some trees however, may require pruning back to facilitate the development, given how close the extension would take the building to the trees along the River Roch. An informative is recommended.

Proximity to River Roch - Whilst no direct impacts would occur to the River Roch, there would be a risk during construction of temporary negative impacts on the ecological potential of the River as defined under the water framework Directive. This risk however, could be removed through a construction and environmental management plan which can be included as a condition of the application requiring a method statement to protect the River Roch to be submitted prior to commencement of development.

Response to objectors -

- Access to the side of No 2 Lawrence Street would not be impeded by the proposed development and access through the gate at the side of their property would be retained.
- Revised plans have been submitted to remove a car display space to enable a delivery vehicle to manoeuvre within the site.
- A Construction Management Plan would be required by condition and to be submitted prior to the commencement of development.
- Details of any additional lighting would be required by condition.
- Issues raised with regard to traffic, access, parking provision and noise have been covered in the above report.

Statement in accordance with Article 35(2) Town and Country Planning (Development Management Procedure) (England) (Amendment) Order 2015

The Local Planning Authority worked positively and proactively with the applicant to identify various solutions during the application process to ensure that the proposal comprised sustainable development and would improve the economic, social and environmental conditions of the area and would accord with the development plan. These were incorporated into the scheme and/or have been secured by planning condition. The Local Planning Authority has therefore implemented the requirement in Paragraphs 186-187 of the National Planning Policy Framework.

Recommendation: Approve with Conditions

Conditions/ Reasons

1. The development must be begun not later than three years beginning with the date of this permission.
Reason. Required to be imposed by Section 91 Town & Country Planning Act 1990.
2. This decision relates to drawings numbered P-0001; P-0010; P-0100; P1010 Rev B; P-1100; P-0110; P-1110; P0170; P-1170; P-0120; P1120; 440375/021 Rev A; Bat survey - Preliminary Roost Assessment 14th November 2016; and the development shall not be carried out except in accordance with the drawings hereby approved.
Reason. For the avoidance of doubt and to ensure a satisfactory standard of design pursuant to the policies of the Bury Unitary Development Plan listed.
3. No development shall commence unless and until:-
 - A contaminated land Preliminary Risk Assessment report to assess the actual/potential contamination and/or ground gas/landfill gas risks at the site shall be submitted to, and approved in writing by, the Local Planning Authority;
 - Where actual/potential contamination and/or ground gas/landfill gas risks have been identified, detailed site investigation and suitable risk assessment shall be submitted to, and approved in writing by the Local Planning Authority;
 - Where remediation/protection measures is/are required, a detailed Remediation Strategy shall be submitted to, and approved in writing by, the Local Planning Authority.Reason. The scheme does not provide full details of the actual contamination and subsequent remediation, which is required to secure the satisfactory development of the site in terms of human health, controlled waters, ground gas and the wider environment and pursuant to National Planning Policy Framework Section 11 - Conserving and enhancing the natural environment.
4. Following the provisions of Condition 4 of this planning permission, where remediation is required, the approved Remediation Strategy must be carried out to the satisfaction of the Local Planning Authority within agreed timescales; and A Site Verification Report detailing the actions taken and conclusions at each stage of the remediation works, including substantiating evidence, shall be submitted to and approved in writing by the Local Planning Authority prior to the development being brought into use.
Reason. To secure the satisfactory development of the site in terms of human health, controlled waters and the wider environment and pursuant to National Planning Policy Framework Section 11 - Conserving and enhancing the natural environment.
5. No development, site clearance or earth moving works shall commence, and no material or machinery shall be brought onto site unless and until a method statement to protect the River Roch from accidental spillages, dust and debris has been submitted to and approved by the Local Planning Authority. All measures shall be implemented and maintained for the duration of the construction period and in accordance with the approved details.
Reason. Information has not been submitted at application stage. To ensure protection of the natural environment pursuant to EN6 - Conservation of the Natural Environment, EN6/3 - Features of Ecological value and chapter 11 - Conserving and enhancing the natural environment of the NPPF.
6. The development hereby approved shall be carried out in accordance with the submitted Flood Risk Assessment (Ref No. SJE/SH/40375-001 dated September 2016) which was prepared by Eastwood partners. No surface water shall be permitted to drain directly or indirectly into the public sewer.
Reason. To ensure a satisfactory form of development and to prevent an undue increase in surface water run-off, to reduce the risk of flooding and to promote

sustainable development pursuant to chapter 10 - meeting the challenge of climate change, flooding and coastal change of the NPPF.

7. No development shall commence unless and until details of the noise output of the plant equipment and workshop related equipment to be used and installed on the site has been submitted to and approved by the Local Planning Authority. The approved details only shall be implemented and thereafter maintained.
Reason. To safeguard the amenities of the occupiers of nearby residential accommodation pursuant to Bury Unitary Development Plan Policies EN1/2 - Townscape and Built Design, EN7/2 - Noise Pollution and S4/4 - Car Showrooms, Car Sales Areas and Petrol Filling Stations.
8. Details/Samples of the materials to be used in the external elevations, together with details of their manufacturer, type/colour and size, shall be submitted to and approved in writing by the Local Planning Authority before the development is commenced. Only the approved materials shall be used for the construction of the development.
Reason. No material samples have been submitted and are required in the interests of visual amenity and to ensure a satisfactory development pursuant to UDP Policy EN1/2 - Townscape and Built design .
9. No development shall commence unless and until details of surface water drainage proposals have been submitted to and approved by the Local Planning Authority. The proposed scheme must be based on the hierarchy of drainage options in the National Planning Practice Guidance and include assessment of potential SuDS options for surface water drainage with appropriate calculations and test results to support the chosen solution. Details of proposed maintenance arrangements should also be provided. The approved scheme only shall be implemented and thereafter maintained.
Reason. The current application contains insufficient information regarding the proposed drainage scheme to fully assess the impact in the interests of sustainable development pursuant to chapter 10 - Meeting the challenge of climate change, flooding and coastal change of the NPPF.
10. The landscaping scheme shown on approved site plan ref P-1010 Revision B shall be implemented within 28 days of the completion of the development hereby approved. Any trees or shrubs removed, dying or becoming severely damaged or becoming severely diseased within five years of planting shall be replaced by trees or shrubs of a similar size or species to those originally required to be planted to the written satisfaction of the Local Planning Authority.
Reason. To secure the satisfactory development of the site and in the interests of visual amenity pursuant to Policy EN1/2 - Townscape and Built Design and EN8/2 – Woodland and Tree Planting of the Bury Unitary Development Plan.
11. No development shall commence unless and until details of lighting have been submitted to and approved by the Local Planning Authority. The approved details only shall be implemented and thereafter maintained.
Reason. In the interests of residential amenity pursuant to Bury Unitary Development Plan Policies EC6/1 - Assessing New Business, Industrial and Commercial Development, EN1/2 - Townscape and Built Design and S4/4 - Car Showrooms, Car Sales Areas and Petrol Filling Stations.
12. No development shall commence unless and until a 'Construction Traffic Management Plan' (CTMP), has been submitted to and approved in writing with the Local Planning Authority and shall confirm/provide the following:
 - Hours of operation and number of vehicle movements;

- Arrangements for the turning and manoeuvring of vehicles within the curtilage of the site;
- Parking on site or on land within the applicant's control of operatives' and demolition/construction vehicles together with storage on site of demolition/construction materials.

The approved plan shall be adhered to throughout the demolition/construction period and the measures shall be retained and facilities used for the intended purpose for the duration of the demolition/construction period. The areas identified shall not be used for any other purposes other than the turning/parking of vehicles and storage of demolition/construction materials.

Reason. Insufficient information has been submitted at application stage. To mitigate the impact of the construction traffic generated by the proposed development on the adjacent residential streets and ensure adequate off street car parking provision and materials storage arrangements for the duration of the construction period, in the interests of highway safety pursuant to Bury Unitary Development Plan Policies EC6/1 - Assessing New Business, Industrial and Commercial Development, EN1/2 - Townscape and Built Design and S4/4 - Car Showrooms, Car Sales Areas and Petrol Filling Stations.

13. No development shall commence unless and until details have been submitted to and approved by the Local Planning Authority to cover measures to ensure that all mud and other loose materials are not carried on the wheels and chassis of any vehicles leaving the site and measures to minimise dust nuisance caused by the operations. The approved details shall be implemented and maintained thereafter during the period of demolition/construction.

Reason. Insufficient information submitted at application stage. To ensure that the adopted highways are kept free of deposited material from the ground works operations pursuant to Bury Unitary Development Plan Policy EN1/2 - Townscape and Built Design.

14. The turning facilities indicated on approved plan reference P-1010 Revision B shall be provided before the development is brought into use and the areas used for the manoeuvring of all service vehicles and car transporters associated with the proposed development within the curtilage of the site shall subsequently be maintained free of obstruction at all times.

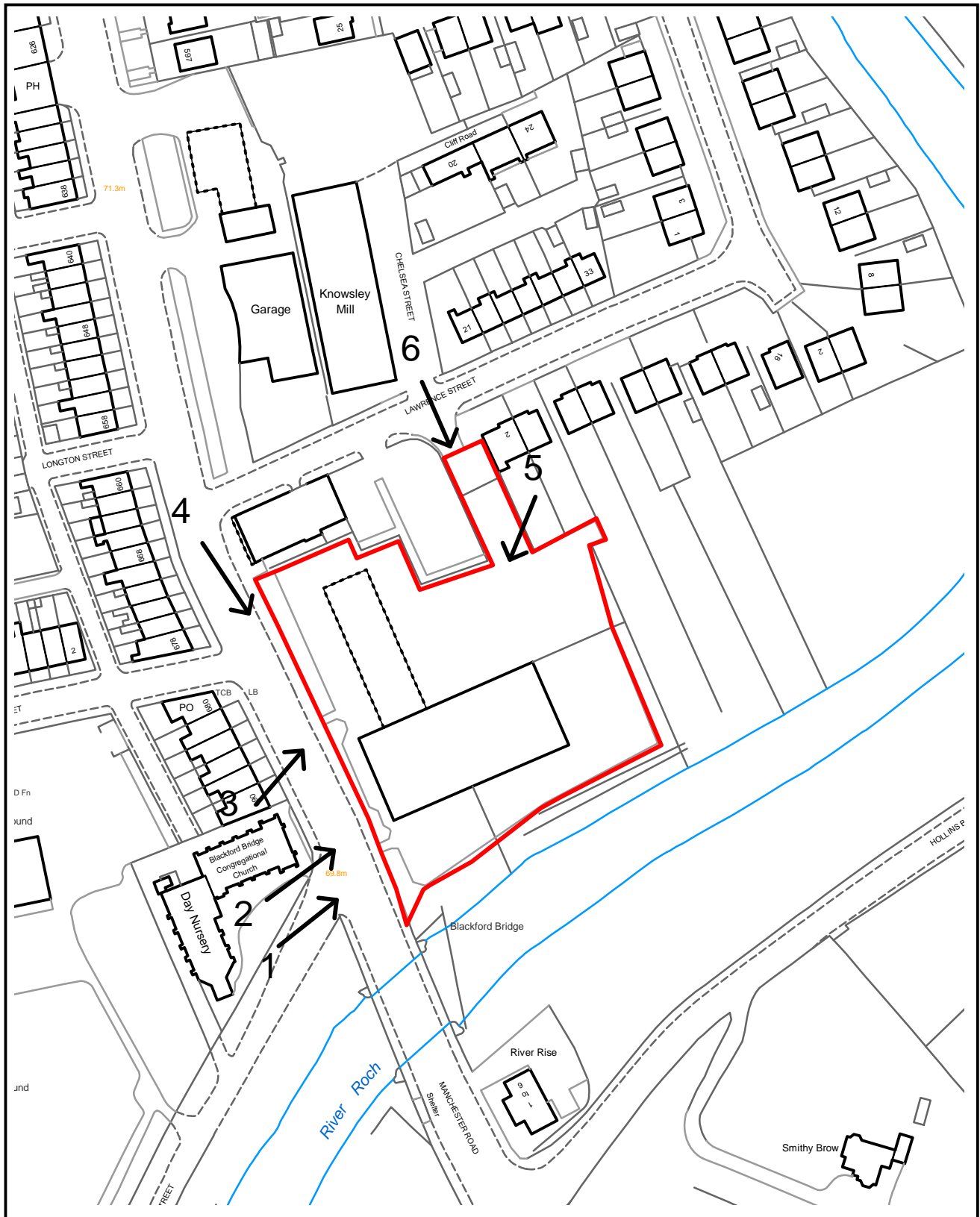
Reason. To minimise the standing and turning movements of vehicles on the highway in the interests of road safety pursuant to Bury Unitary Development Plan Policies EC6/1 - Assessing New Business, Industrial and Commercial Development, S4/4 - Car Showrooms, Car Sales Areas and Petrol Filling Stations and HT6/2 - Pedestrian Vehicular Conflict.

15. The customer, staff (general), staff (demonstrator vehicles), service and 'stock & storage' car parking indicated on approved plan reference P-1010 Revision B shall be surfaced, demarcated and made available prior to the development hereby approved being brought into use and for the intended respective uses identified only. The parking provision shall thereafter be maintained available for the intended respective uses at all times.

Reason. To ensure adequate off street car parking provision in the interests of road safety pursuant to policies EC6/1 - Assessing New Business, Industrial and Commercial Development, S4/4 - Car Showrooms, Car Sales Areas and Petrol Filling Stations and HT2/4 - Car Parking and New Development of the Bury Unitary Development Plan.

For further information on the application please contact **Jennie Townsend** on **0161 253-5320**

Viewpoints



PLANNING APPLICATION LOCATION PLAN

APP. NO 60732

**ADDRESS: Hatfields
Manchester Road
Bury**

Planning, Environmental and Regulatory Services



Bury
COUNCIL

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60732

Photo 1



Photo 2



Photo 3



Photo 4



Photo 5



Photo 6





Revisions		
No.	Date	Issue Notes

RITE AREA - 6830sq.m (0.68 hectares / 1.47 acres)

EXISTING BUILDING FOOTPRINT - 1305sq.m

INCLUDING

- MAIN SHOWROOM DISPLAY & ADMIN
- SHOWROOM EXTENSION
- WORKSHOP, MOT & AFTERSALES SERVICES

EXISTING PARKING SCHEDULE

- CUSTOMER PARKING (incl Disabled bay) - 10
- GENERAL FORECOURT DISPLAY - 54
- SERVICE WIP & STAFF PARKING - 18
- STOCK / STORAGE - 40

APPROX. TOTAL PARKING BAYS - 122

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Dimensions must be checked on site before work commences.
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HATFIELDS
VRE BURY
MANCHESTER ROAD
BURY, BL9 9ST

**EXISTING
SITE PLAN**

Project: YOL 338 Scale: 1:200@A1 1:400@A3
Client: EA Project: AO Date: 01 OCT '16

Drawing: P-0010
of
PLANNING
VRE_BURY-PLN

01 EXISTING SITE PLAN
Scale: 1:200@A1 1:400@A3
0 20 M



01 PROPOSED SITE PLAN
Scale: 1:200@A1 1: 400@A3

0 20 M

Revisions		
No.	Date	Issue Notes
A	08/12/16	Fencing and gates amended to allow clear transporter access on site. Cycle Shelter added.
B	23/12/16	Staff parking bays increased to 30no. on site

SITE AREA - 5930sq.m / 0.59 hectares / 1.47 acres

PROPOSED BUILDING FOOTPRINT - 1475sq.m INCLUDING

- NEW main showroom display & admin (to replace existing)
- showroom extension (retained)
- Workshop, Parts & aftersales services (refurbished)
- NEW extension for MOT and Valet services

PROPOSED PARKING SCHEDULE	
Customer parking (Sales)	
(incl Disabled and Parent & Child bay)	- 12
Customer parking (Service / Aftersales)	- 5
Staff parking bays (Forecourt)	
(designated for Demonstrations & display)	- 10
Staff parking bays (rear, General)	- 20
General 'SELEKT' forecourt display	- 45
Service WIP & Valet Prep bays	- 10
Stock / Storage	- 20
APPROX. TOTAL PARKING BAYS	- 122

EXTERNAL SURFACE FINISHES	
ST-01: BLACK ASPHALT (TARMAC)	General tarmac, with graphic marking applied in white asphalt paint. To entire external surface unless indicated;
ST-02: BLOCK PAVING - SILVER FINISH (light grey)	Tobermore 'Fusion', colour - Silver; 200 x 100mm, 80mm thick block paving, pattern; 90 deg stretcher bonds. To forecourt 'PLAZA' area and customer parking areas
ST-03: BLOCK PAVING - GRAPHITE FINISH (dark grey)	Tobermore 'Fusion', colour - Graphite; 200 x 100mm, 80mm thick block paving, pattern; 90 deg stretcher bonds. To demarcate 'Selekt' customer parking, demo display and service parking bays, and building perimeter paving apron
ST-04: PEBBLES BED	'Marshalls' Atlantic pebbles in planter beds with planting where indicated. To front of Shop Windows, and forecourt area parking
ST-05: BRUSHED CONCRETE	Brushed concrete, cast in situ, with inserted uplighters where indicated. To handstanding areas and Vehicle display Podium on forecourt

BOUNDARY / PERIMETER FENCING	
PF-01: HOOPS BOLLARDS	1400mm Rhino Hoop Barrier by Marshalls Ltd. RB80 or equal approved. Finish - RAL 9006, silver metallic, as existing. As required to secure forecourt perimeter and display along Manchester Road
PF-02: REMOVABLE BOLLARDS	Rhino Protec Removable Bollard by Marshalls Ltd. RT/SSS or equal approved; Finish - Stainless Steel. To vehicle access points in and out of the site
PF-03: FIXED BOLLARDS	1000mm high Rhino Fixed Bollard by Marshalls Ltd. RS001 or equal approved; Finish - Stainless Steel. To vulnerable areas along the building perimeter susceptible to vehicle damage
PF-04: SLOPED-TOP PARKING BOLLARDS	153mm diameter, 1000mm high ZEN 451 sloped top stainless steel bollards by 'Zenith', incorporated bay parking sign (blue RAL 5002 top, Volvo Sans Font, with white logo relay). Installed at front of customer parking bays
PF-05: PALISADE FENCING	Steel Palisade fencing, Finish - Galvanised Steel finish, as existing. To perimeter boundaries as existing, bin stores & to secure vehicle storage with gates at rear

SOFT LANDSCAPING	
SL - SOFT LANDSCAPING & PLANTERS	New shrubs and grass mix landscaping to forecourt and planter beds. No new trees proposed, existing rear perimeter trees retained. Species and Planting schedule of native buffer mix, and low level shrubs, to match existing. (Landscaping Plan & Details to be finalised)

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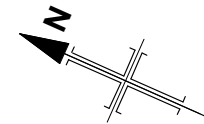
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Stevenage, Oxfordshire
OX13 6SW, United Kingdom

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Client	HATFIELDS		
Project Title	VRE BURY MANCHESTER ROAD BURY, BL9 9ST		
Drawing Title	PROPOSED SITE PLAN		
Project ID	VOL 338	Scale	1:200@A1 1:400@A3
Drawn By	EA	Reviewed By	A0 Date 03 OCT '16
Drawing No.	P-1010	Revision	B
of		CAD File Name	
PLANNING		VRE_BURY-PLNG	



01 PROPOSED GROUND FLOOR PLAN
Scale: 1:100@A1 1: 200@A3



Revisions		
No.	Date	Issue Notes

PROPOSED FLOOR AREA SCHEDULES (GEA) sq.m

TOTAL PROPOSED GROUND FLOOR	- 1580
- NEW Main Showroom display & Admin	- 600
- Showroom Extension (retained)	- 240
- Workshop, Parts & aftersales services (refurbished)	- 625
- NEW MOT and Valet Bays extension	- 115
.....

TOTAL PROPOSED FLOOR AREAS (GEA) - 1980	
Total proposed Ground Floor	- 1580
Total proposed First Floor	- 400

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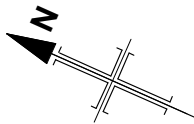
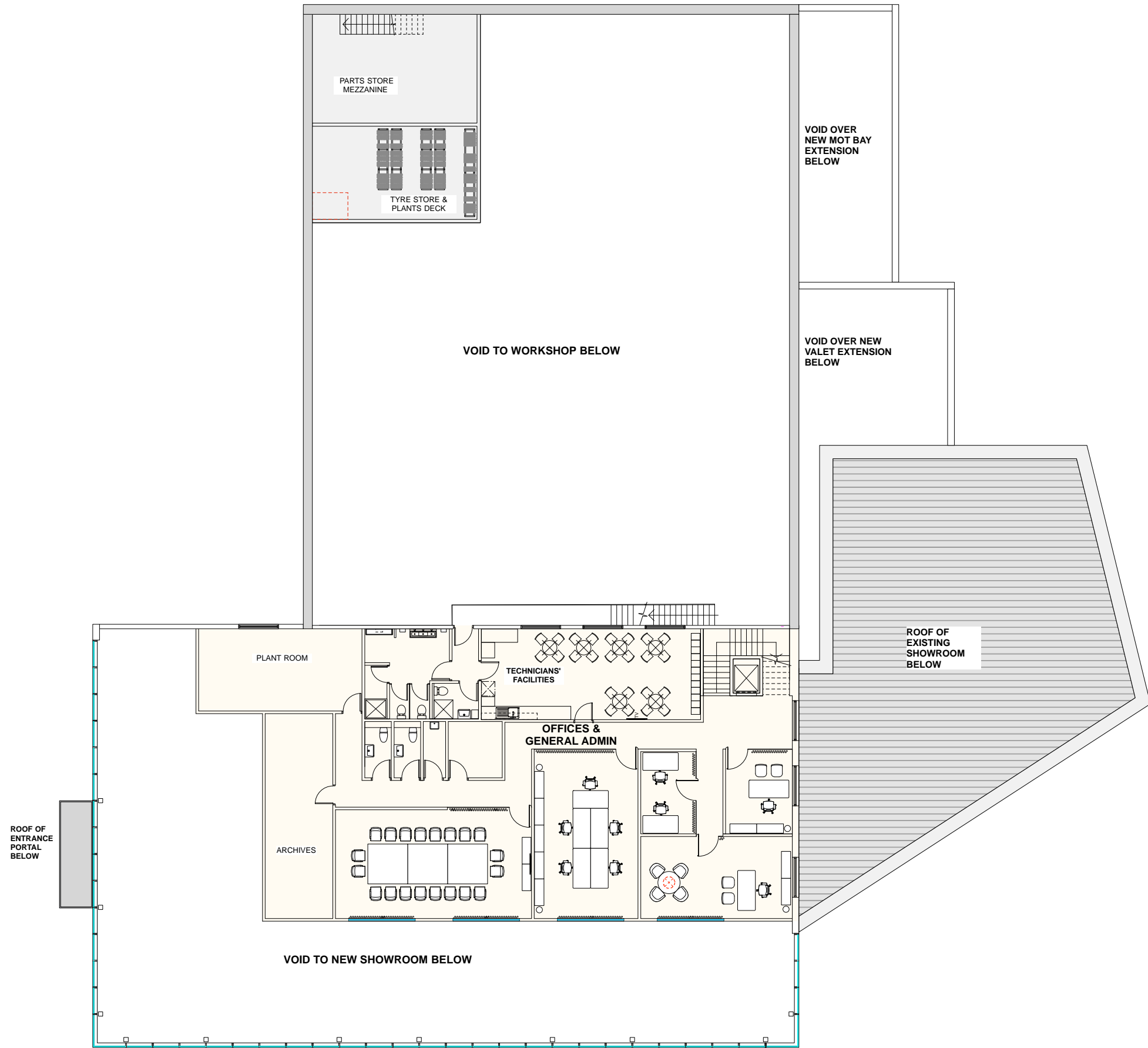
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Client	HATFIELDS
Project Title	VRE BURY MANCHESTER ROAD BURY, BL9 9ST

Drawing Title	PROPOSED GROUND FLOOR PLAN
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Project ID	VOL 338		Scale	1:100@A1 1:200@A3	
Drawn By	EA		Reviewed By	AO	Date 01 OCT '16
Drawing No.				Revision	

of	PLANNING	CAD File Name	VRE_BURY-PLNG
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Revisions		
No.	Date	Issue Notes

PROPOSED FLOOR AREA SCHEDULES (GEA) sq.m

TOTAL PROPOSED FIRST FLOOR	- 400
- Offices, Staff Facilities & Gen Admin	- 330
- Parts Mezzanine & Tyre Deck	- 70

TOTAL PROPOSED FLOOR AREAS (GEA) - 1980	
Total proposed Ground Floor	- 1580
Total proposed First Floor	- 400

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Client	HATFIELDS
Project Title	VRE BURY MANCHESTER ROAD BURY, BL9 9ST

Drawing Title	PROPOSED FIRST FLOOR PLAN
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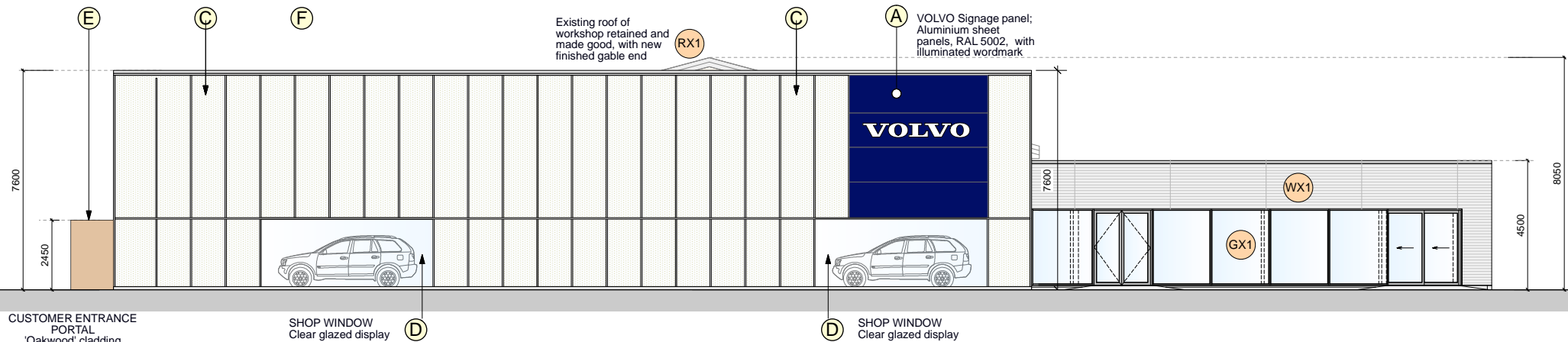
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VOL 338	1:100@A1	1:200@A3
Drawn By	Reviewed By	Date
EA	AO	01 OCT '16

of	PLANNING	CAD File Name	VRE_BURY-PLNG
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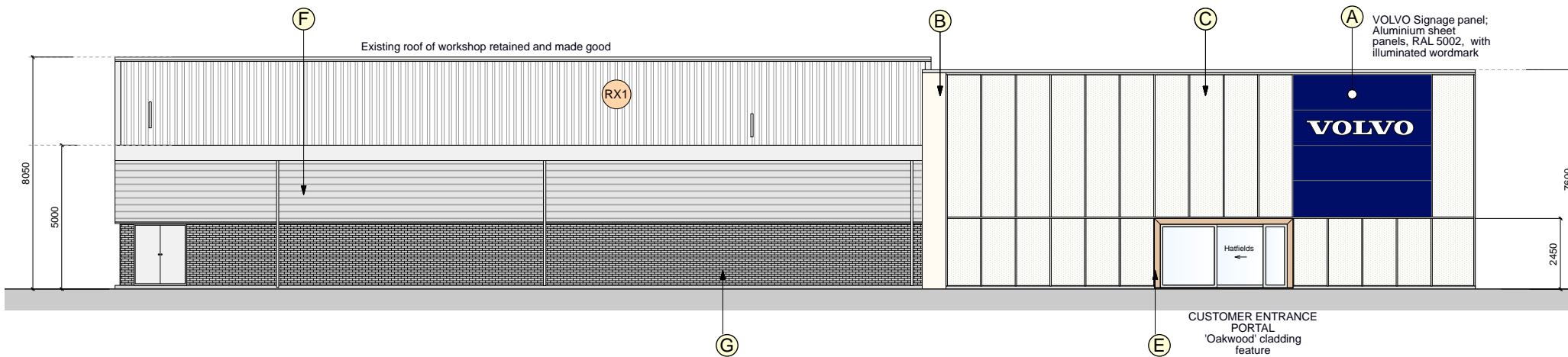
01 PROPOSED FIRST FLOOR PLAN
Scale: 1:100@A1 1: 200@A3



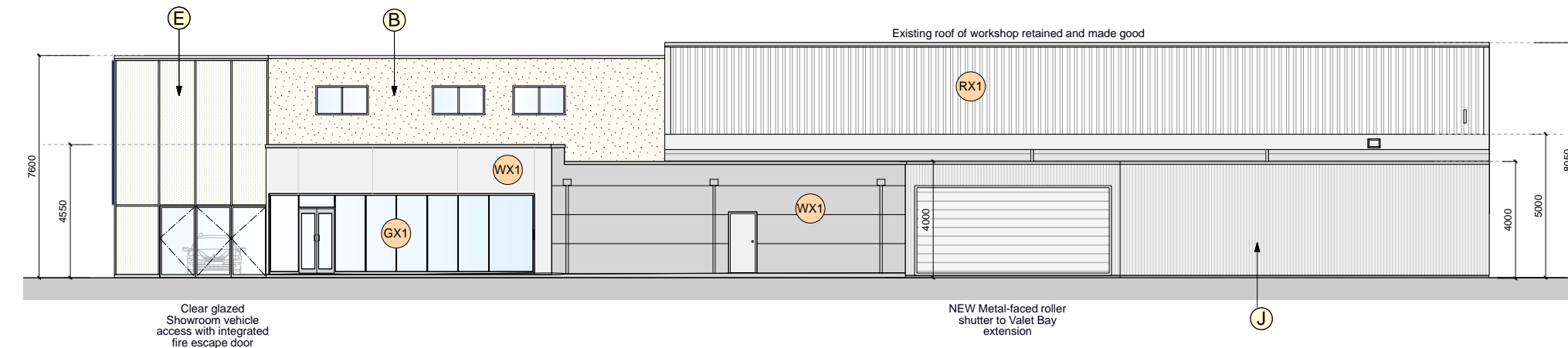
01 PROPOSED FRONT (SW) ELEVATION
Scale: 1:100@A1 1: 200@A3



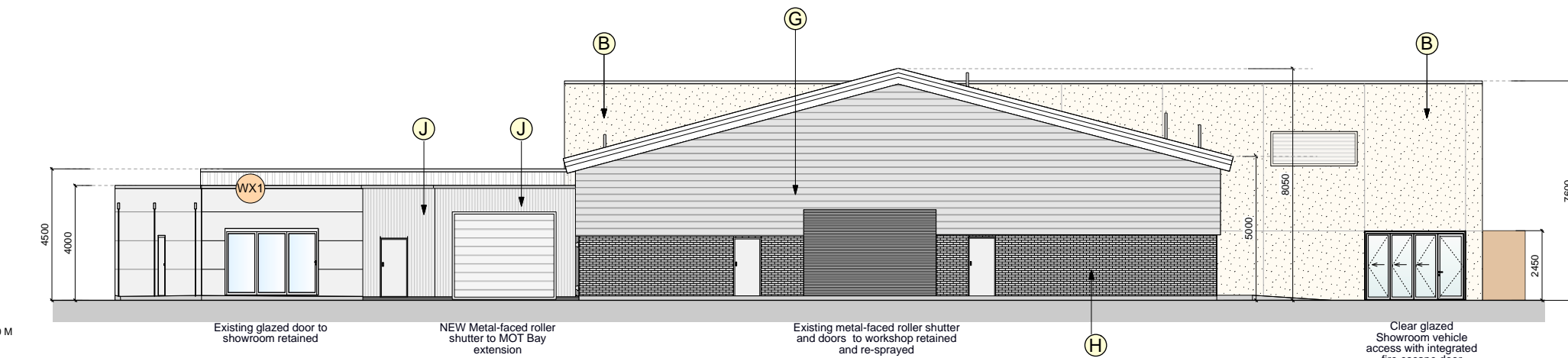
02 PROPOSED LH SIDE (NW) ELEVATION
Scale: 1:100@A1 1: 200@A3



03 EXISTING RH SIDE (SE) ELEVATION
Scale: 1:100@A1 1: 200@A3



04 EXISTING REAR (NE) ELEVATION
Scale: 1:100@A1 1: 200@A3



Revisions		
No.	Date	Issue Notes

FACADE TREATMENT MATERIALS LEGEND



- RX1 Existing workshop corrugated roof retained, and made good. New gable end finished with metal sheets to match walls
- WX1 Existing wall finish of showroom extension; silver cladding panels, retained and made good where required
- GX1 Existing showroom extension glazing; Glazing retained, aluminium frames to be re-sprayed silver RAL 9006 to match new showroom glazing. No film applied internally

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Client **HATFIELDS**

Project Title **VRE BURY
MANCHESTER ROAD
BURY, BL9 9ST**

Drawing Title **PROPOSED
EXTERNAL ELEVATIONS &
FACADE TREATMENT**

Project ID **VOL 338** Scale **1:100@A1 1:200@A3**

Drawn By **EA** Reviewed By **AO** Date **01 OCT '16**

Drawing No. **P-1170** Revision **-**

PLANNING

CAD File Name
VRE_BURY-PLNG

Ward: Radcliffe - North

Item 07

Applicant: Mr Chris Pearson

Location: 41 Bury Old Road, Ainsworth, Bolton, BL2 5PF

Proposal: Change of use of stable to dwelling with single storey extension

Application Ref: 60784/Full

Target Date: 10/01/2017

Recommendation: Approve with Conditions

Description

The application site is within the grounds of 41 Bury Old Road, which is a semi-detached property situated in an isolated position within open land in the Green Belt. The site is to the west of Ainsworth Village and is close to the junction of Bury Old Road and Arthur Lane.

41 Bury Old Road is a semi-detached property, which has a small holding of approx 0.5 hectares to the rear and side. The small holding is located close to the junction of Arthur Lane and Bury Old Road.

There are a series of buildings located to the east of the dwelling. There is a timber shed and shelter along the boundary with Bury Old Road and these are screened from view by a 3 metre high hedge. To the north of this is a hardstanding with a square building beyond, which is currently used as a garage and stables. To the east of this is a store. There is a further brick building at the northern corner of the site, which has been converted to stables and is the subject of this application.

The proposed development involves the conversion of the stables building to a dwelling with a single storey side extension. The proposed single storey extension would be located on the western elevation and would measure 3 metres by 5 metres. The proposed building would be rendered and a Decra tile effect lightweight roofing system would be used. Access would be taken from the existing access onto Arthur Lane and parking would be provided to the north of the building for 4 vehicles.

Relevant Planning History

37444 - Change of use of land to manege at 41 Bury Old Road, Ainsworth. Approved with conditions - 15 June 2001.

45805 - Two storey extension at side at 41 Bury Old Road, Ainsworth. Approved with conditions - 10 March 2006.

46956 - Revised roof line and 2 dormers (retrospective) at 41 Bury Old Road, Ainsworth. Approved with conditions - 30 October 2006

50298 - Replacement of existing building (partly retrospective) at 41 Bury Old Road, Ainsworth. Approved with conditions - 22 October 2008

50878 - Demolition and replacement of existing stables; erection of building containing stables and tack room at 41 Bury Old Road, Ainsworth. Refused - 23 March 2009

51655 - Renovation of existing stables (re-submission of planning application 50878) at 41 Bury Old Road, Ainsworth. Approved with conditions - 10 September 2009.

53423 - Conversion of existing building to boarding cattery with external exercise area at 41

Publicity

The neighbouring properties were notified by means of a letter on 15 November and site notices were posted on 17 November 2016.

2 letters of support have been received from the occupiers of Brown Hill Boarding Kennels and Greenbank Barn, which has raised the following issues:

- I have lived in the village for 17 years and know the site well.
- Would like to support the application as it is well placed and will not pose a problem for traffic.
- The traffic for a house will be less than for the stables.
- The application seems tasteful and in keeping with the area.
- There are plenty of stables in the area and there are some vacant ones.
- The building is set back off the road and there is a good view out of the drive to the road side both ways.

1 letter of objection has been received from the Ainsworth Community Association, which has raised the following issues:

- Wish to object on the grounds of inappropriate development within the Green Belt and West Pennine Moors Area.
- The building should be retained for use as a stable for which there is demand in the area.
- The site is agricultural land and some has already been converted into a garden next to the stable.
- There would be problems with additional traffic near to a dangerous road junction.

The supporters and objector have been notified of the Planning Control Committee meeting.

Consultations

Traffic Section - No objections in principle. Further comments to be reported in the Supplementary Report.

Drainage Section - Comments awaited.

Environmental Health - Contaminated Land - No objections, subject to the inclusion of conditions relating to contaminated land.

Waste Management - Comments awaited.

United Utilities - Comments awaited

The Coal Authority - No objections, subject to the inclusion of an informative relating to coal mining.

GM Ecology Unit - No objections, subject to the inclusion of a condition relating to landscaping and an informative relating to bats.

Unitary Development Plan and Policies

H1/2	Further Housing Development
H2/1	The Form of New Residential Development
H2/2	The Layout of New Residential Development
EN1/1	Visual Amenity
EN1/2	Townscape and Built Design
EN6	Conservation of the Natural Environment
EN6/3	Features of Ecological Value
EN7	Pollution Control
EN7/5	Waste Water Management
EN9/1	Special Landscape Areas
OL1/4	Conversion and Re-use of Buildings in the Green Belt
HT2/4	Car Parking and New Development
SPD6	Supplementary Planning Document 6: Alterations & Extensions
SPD 9	Conversion and Re-use of Buildings in the Green Belt
SPD11	Parking Standards in Bury
NPPF	National Planning Policy Framework

Issues and Analysis

The following report includes analysis of the merits of the application against the relevant policies of both the National Planning Policy Framework (NPPF) and the adopted Bury Unitary Development Plan (UDP) together with other relevant material planning considerations. The policies of the UDP that have been used to assess this application are considered to be in accordance with the NPPF and as such are material planning considerations. For simplicity, just the UDP Policy will be referred to in the report, unless there is a particular matter to highlight arising from the NPPF where it would otherwise be specifically mentioned.

Principle (Green Belt) - Paragraph 90 of the National Planning Policy Framework (NPPF) states that the re-use of buildings within the Green belt would not be inappropriate development providing that they are of permanent and substantial construction.

Policy H1/2 states that all new residential development should make a positive contribution to the surrounding area and should have regard to the heights and roof types of adjacent buildings, the position and proximity of neighbouring dwellings and the density and character of the surrounding area.

Policy OL1/4 states that the conversion and re-use of buildings within the Green Belt will be permitted providing that the proposal would not have a materially greater impact than the present use upon the openness of the Green Belt; the buildings are capable of conversion without major or complete reconstruction; the form, bulk and general design are in keeping with the surroundings and suitable access and the likely traffic can be accommodated without creating a hazard or the need for major road improvements.

A structural report has been submitted with the application, which confirms that the building is structurally sound and capable of conversion. The issues of form, bulk and design, access and traffic will be discussed later in the report. The proposed use would not have a materially greater impact upon the openness of the Green Belt and as such, would be in accordance with Policies H1/2 and OL1/4 of the Bury Unitary Development Plan and the NPPF.

Design and layout - The proposed development would utilise the existing openings on the southern elevation and would add a further 6 openings in the remainder of the building. The proposed openings would be randomly arranged and the overall design and location of the proposed openings would retain the character and appearance of the existing stables.

The proposed extension would be located on the north western side of the building and would represent a third of the existing building in terms of volume. The proposed extension would be the same height as the existing building and would be rendered to match the existing building. As such, the proposed development would be in keeping with the form and design of the existing building and the simplicity of the building has been retained by keeping the number of new openings to a minimum.

The proposed development would respect the character and appearance of the existing building and would not harm the character or openness of the Green Belt. A condition restricting permitted development rights would ensure that future proposed changes are controlled. The proposed materials would be acceptable and would not be a prominent feature within the streetscene. Therefore, the proposed development would be in accordance with Policies OL1/4 and EN1/2 of the Bury Unitary Development Plan and SPD9 and SPD10.

Impact upon surrounding area - The proposed residential curtilage would be defined by the existing fencing. The existing hawthorn hedge would be retained along the western boundary and the land surrounding the stables would be used as residential curtilage. The proposed site would be viewed against a backdrop of existing buildings. In addition, permitted development rights for outbuildings in the garden area and the extension of the

dwelling would be removed, which would reduce the impact upon the openness and character of the Green Belt. Therefore, the proposed development would not be a prominent feature within the locality and would be in accordance with Policy EN1/2 of the Bury Unitary Development Plan.

Impact upon residential amenity - SPD6 provides guidance on aspect standards between residential properties and is relevant in this case.

There would be 59 metres between the proposed building and No. 39 Bury Old Road and 64 metres between the proposed building and No. 41 Bury Old Road. There would be 61.8 metres between the proposed building and the stable block close to Sunnybank, which is currently being converted into a dwelling. All of these distances would be in excess of the 20 metre aspect standard from SPD6. Therefore, the proposed development would not have an adverse impact upon the amenity of the neighbouring properties.

Bats - A bat survey was submitted with the application. It concluded that the building has negligible potential for roosting bats and there is no foreseeable impact from the development on bats. GM Ecology Unit has no objections, subject to the inclusion of a condition relating to landscaping. Therefore, the proposed development would not cause harm to a protected species and would be in accordance with Policies EN6 and EN6/3 of the Bury Unitary Development Plan and the NPPF.

Highways issues - The proposed development would be accessed from the existing access road and appropriate levels of visibility would be provided. An area of 4.7 metres wide by 5.6 metres in length would be provided adjacent to Arthur Lane, which would be sufficient for two vehicles to pass each other. Turning facilities would be provided within the site and the proposed development would generate less traffic than the existing stables use. The Traffic Section has no objections in principle to the development and further comments will be reported in the Supplementary Report following receipt of revised plans detailing the location of the parking spaces.

Parking - SPD11 states that the maximum parking standards is 1.5 spaces per 2 bed dwelling.

There would be space at the rear of the proposed dwelling to accommodate 2 parking spaces and allow sufficient space for turning. Therefore, the proposed development would comply with the maximum parking standards and the proposed development would be in accordance with Policy HT2/4 of the Bury Unitary Development Plan and SPD11.

Response to objectors

The issues relating to the principle of the development within the Green Belt and traffic have been addressed in the report above.

Statement in accordance with Article 35(2) Town and Country Planning (Development Management Procedure) (England) (Amendment) Order 2015

The proposal complies with the development plan and would improve the economic, social and environmental conditions of the area. It therefore comprises sustainable development and the Local Planning Authority worked proactively and positively to issue the decision without delay. The Local Planning Authority has therefore implemented the requirement in Paragraphs 186-187 of the National Planning Policy Framework.

Recommendation: Approve with Conditions

Conditions/ Reasons

1. The development must be begun not later than three years beginning with the date of this permission.

Reason. Required to be imposed by Section 91 Town & Country Planning Act 1990.

2. This decision relates to drawings numbered 16/262/01, 16/262/02, 16/262/03, 16/262/04 and the development shall not be carried out except in accordance with the drawings hereby approved.
Reason. For the avoidance of doubt and to ensure a satisfactory standard of design pursuant to the policies of the Bury Unitary Development Plan listed.
3. No development shall commence unless and until:-
 - A contaminated land Preliminary Risk Assessment report to assess the actual/potential contamination and/or ground gas/landfill gas risks at the site shall be submitted to, and approved in writing by, the Local Planning Authority;
 - Where actual/potential contamination and/or ground gas/landfill gas risks have been identified, detailed site investigation and suitable risk assessment shall be submitted to, and approved in writing by the Local Planning Authority;
 - Where remediation/protection measures is/are required, a detailed Remediation Strategy shall be submitted to, and approved in writing by, the Local Planning Authority.Reason. The scheme does not provide full details of the actual contamination and subsequent remediation, which is required to secure the satisfactory development of the site in terms of human health, controlled waters, ground gas and the wider environment and pursuant to National Planning Policy Framework Section 11 - Conserving and enhancing the natural environment.
4. Following the provisions of Condition 3 of this planning permission, where remediation is required, the approved Remediation Strategy must be carried out to the satisfaction of the Local Planning Authority within agreed timescales; and A Site Verification Report detailing the actions taken and conclusions at each stage of the remediation works, including substantiating evidence, shall be submitted to and approved in writing by the Local Planning Authority prior to the development being brought into use.
Reason. To secure the satisfactory development of the site in terms of human health, controlled waters and the wider environment and pursuant to National Planning Policy Framework Section 11 - Conserving and enhancing the natural environment.
5. A landscaping scheme shall be submitted to, and approved in writing by, the Local Planning Authority prior to the commencement of the development. It shall be implemented not later than 12 months from the date the building(s) is first occupied; and any trees or shrubs removed, dying or becoming severely damaged or becoming severely diseased within five years of planting shall be replaced by trees or shrubs of a similar size or species to those originally required to be planted to the written satisfaction of the Local Planning Authority.
Reason. To secure the satisfactory development of the site and in the interests of visual amenity pursuant to the following Policies of the Bury Unitary Development Plan:
Policy EN1/2 - Townscape and Built Design
Policy EN6 - Conservation of the Natural Environment
Policy EN8/2 – Woodland and Tree Planting.
6. Details/Samples of the roofing material, together with details of their manufacturer, type/colour and size, shall be submitted to and approved in writing by the Local Planning Authority before the development is commenced. Only the approved materials shall be used for the construction of the development.
Reason. No material samples have been submitted and are required in the interests of visual amenity and to ensure a satisfactory development pursuant to UDP Policy EN1/1 Visual Amenity.
7. The external finishing materials for the extension hereby approved shall match

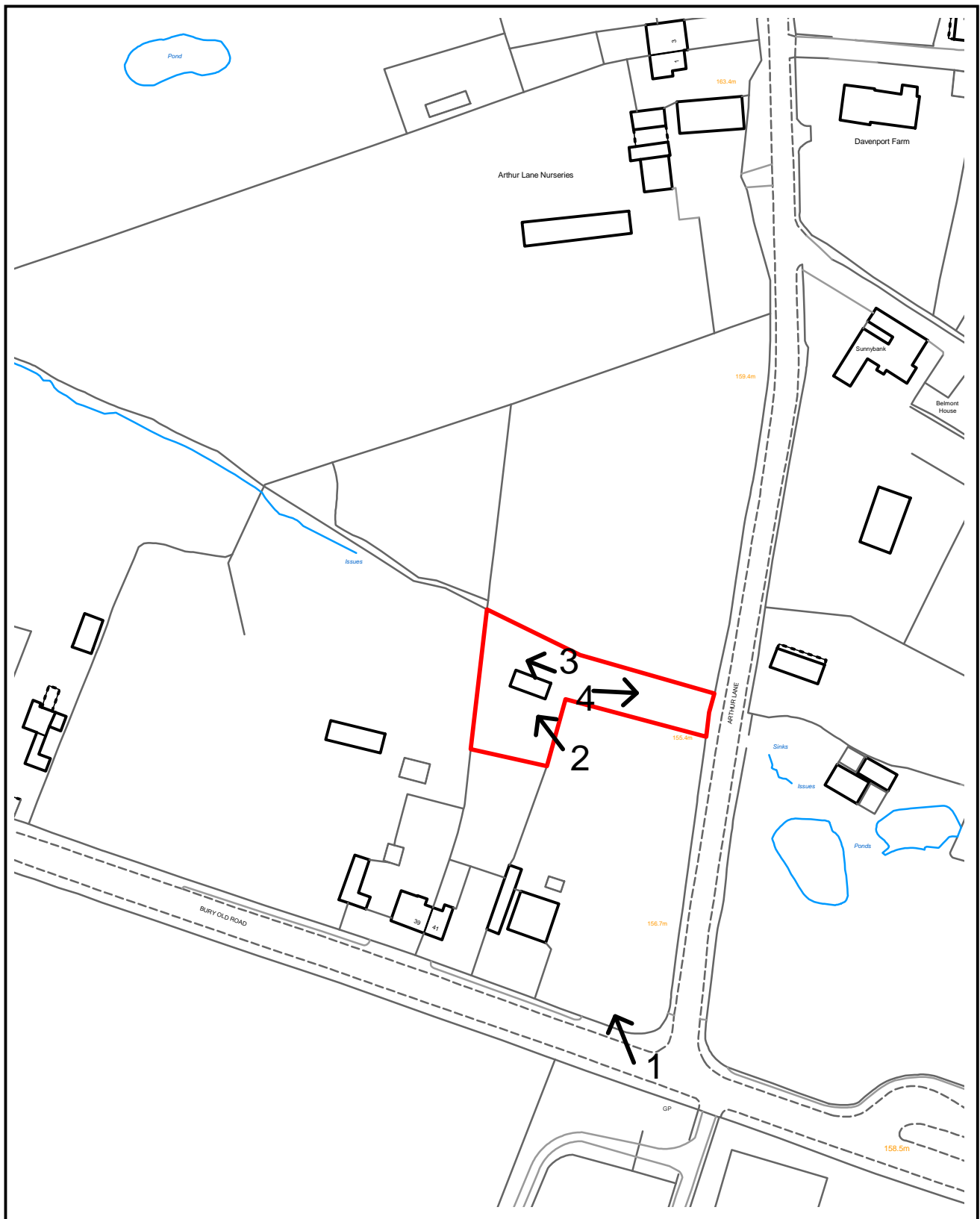
those of the existing building.

Reason. In the interests of visual amenity and to ensure a satisfactory development pursuant to Policy EN1/2 - Townscape and Built Design of Bury Unitary Development Plan.

8. Notwithstanding the terms of the Town and Country Planning (General Permitted Development) (England) Order 2015, as subsequently amended, no development shall be carried out within the terms of Classes A to E of Part 1 of Schedule 2 of the Order, without the submission and approval of a relevant planning application.
Reason. To ensure that future inappropriate alterations or extensions do not occur pursuant to policies of the Unitary Development Plan listed.

For further information on the application please contact **Helen Longworth** on **0161 253 5322**

Viewpoints



PLANNING APPLICATION LOCATION PLAN

APP. NO 60784

**ADDRESS: 41 Bury Old Road
Ainsworth**

Planning, Environmental and Regulatory Services

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Bury
COUNCIL

60784

Photo 1



Photo 2



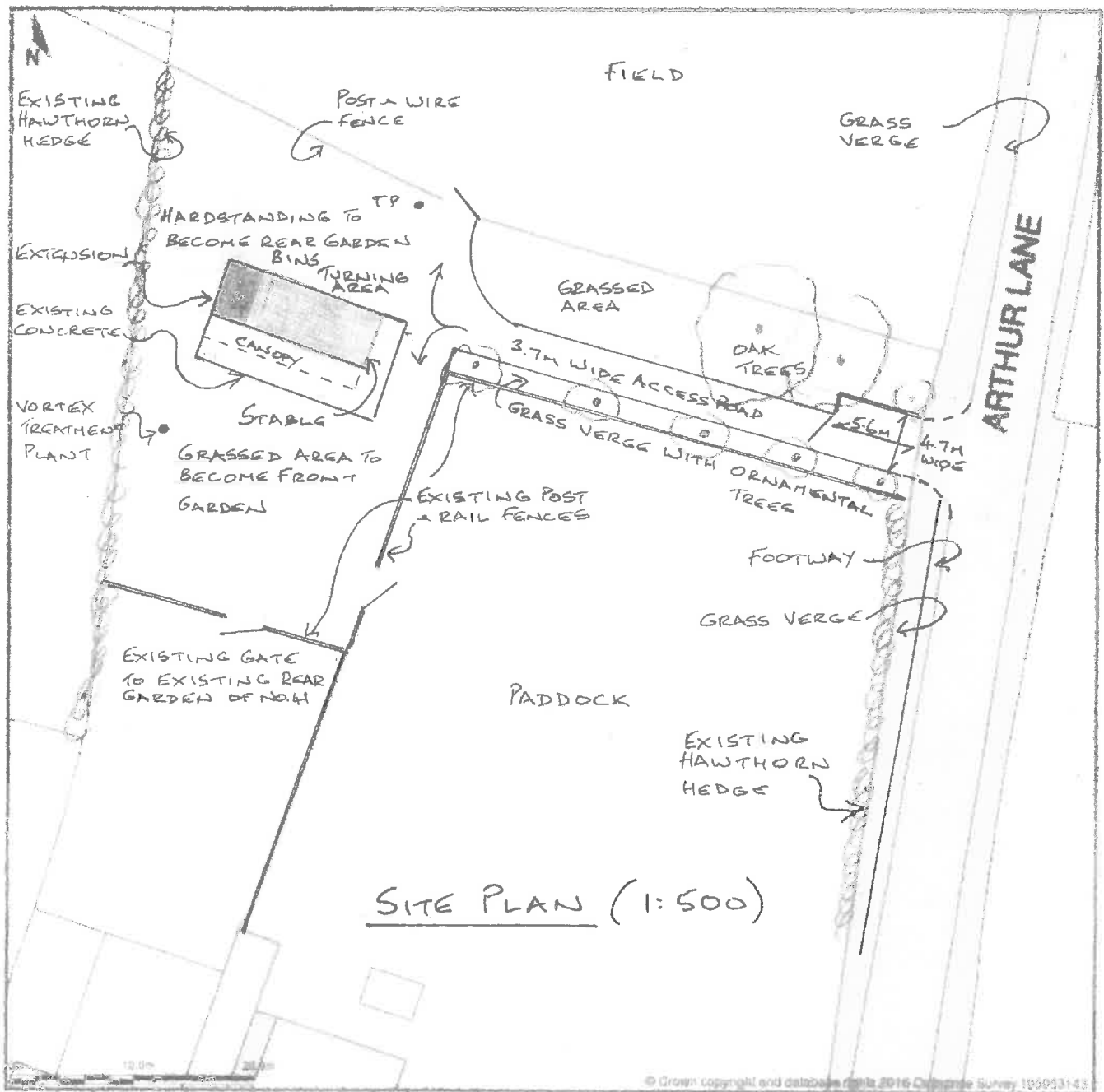
Photo 3



Photo 4



41 Bury Old Road, Bolton, BL2 5PF



Block Plan shows area bounded by: 375790.34, 410323.71 375880.34 410413.71 (at a scale of 1:500). The representation of a road, track or path is no evidence of a right of way. The representation of features as lines is no evidence of a property boundary.

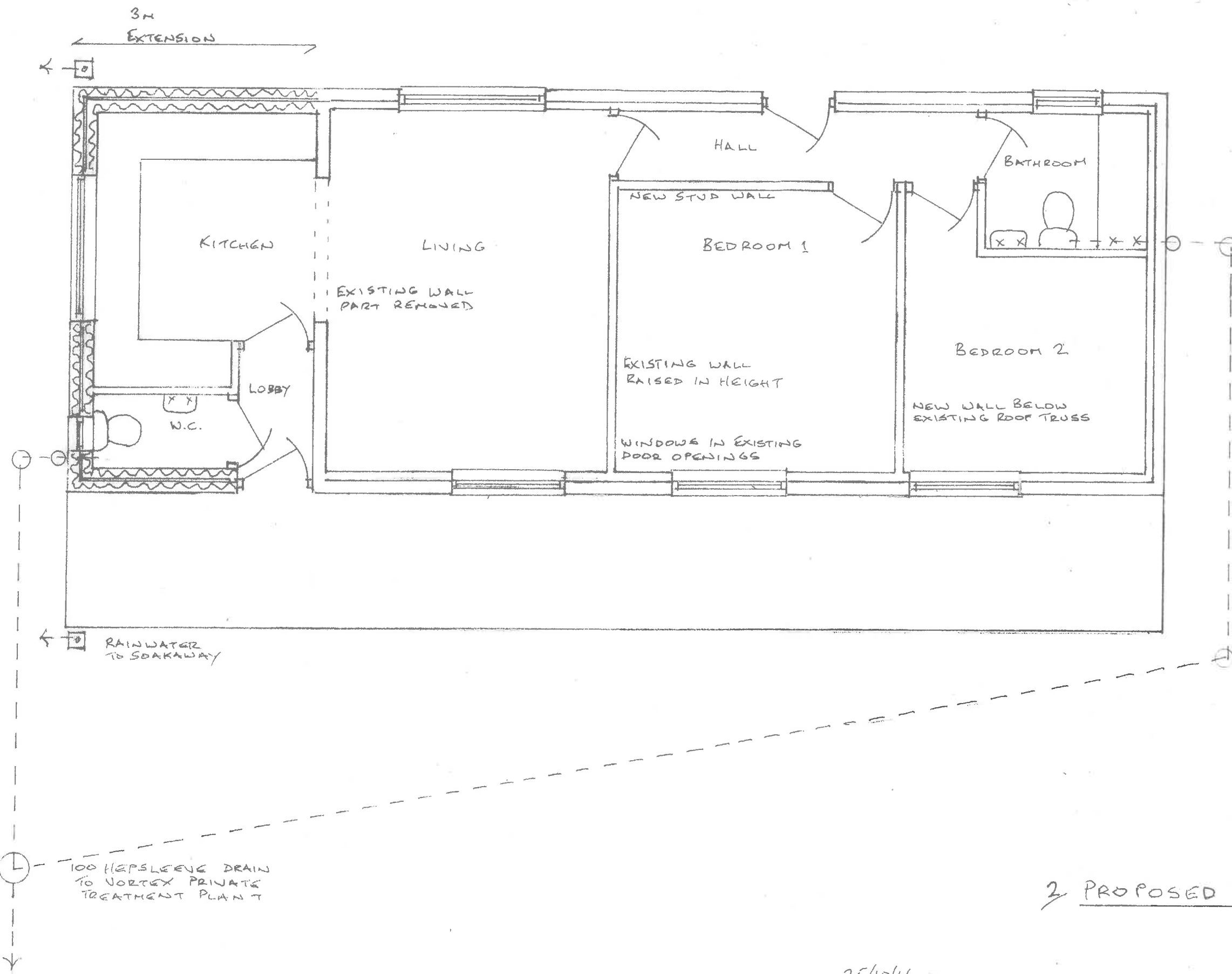
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CONVERSION OF STABLES TO DWELLING
REAR 41, BURY OLD ROAD, AINSWORTH BL2 5PF

25/10/16

16/262/04



2 PROPOSED PLAN

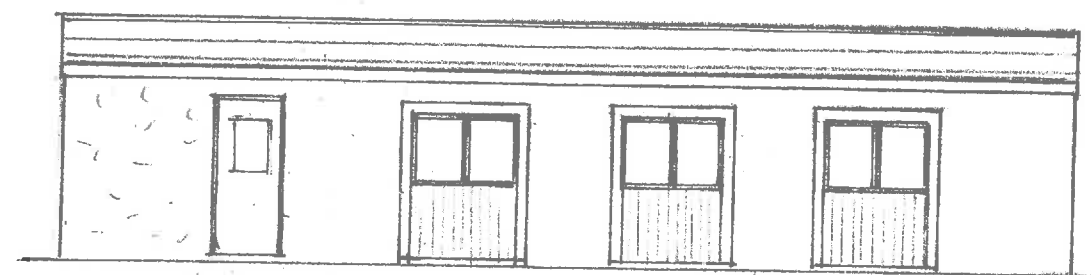
SCALE 1:50 @ A3

25/10/16

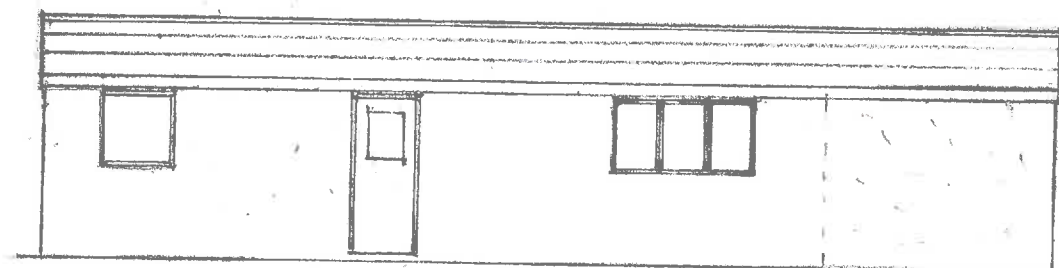
16/262/02

CONVERSION OF STABLE TO DWELLING

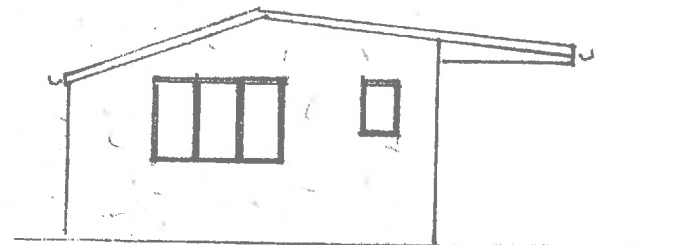
REAR 41, BURY OLD ROAD, AINSWORTH BL2 5PF



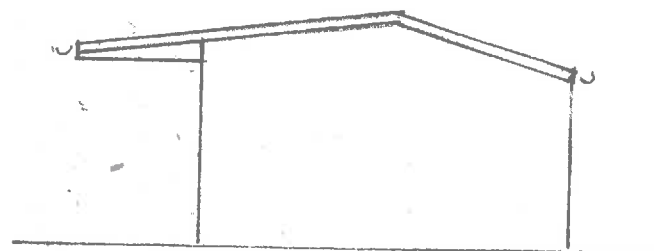
FRONT (SOUTH) ELEVATION



REAR ELEVATION



SIDE



SIDE (UNCHANGED)

SCALE 1:100 @ A3

DECRA TILE EFFECT LIGHTWEIGHT ROOFING
SYSTEM - COLOUR DARK GREY

EXISTING RENDERED WALLS RETAINED.
EXTENSION RENDERED TO MATCH.

EXISTING STABLE DOOR OPENINGS & TIMBER
SURROUNDS RETAINED. BROWN UPVC OR
TIMBER WINDOWS, VERTICAL BOARDING BELOW
TO REPLICATE STABLE DOORS



LOCATION PLAN
(1:1250)

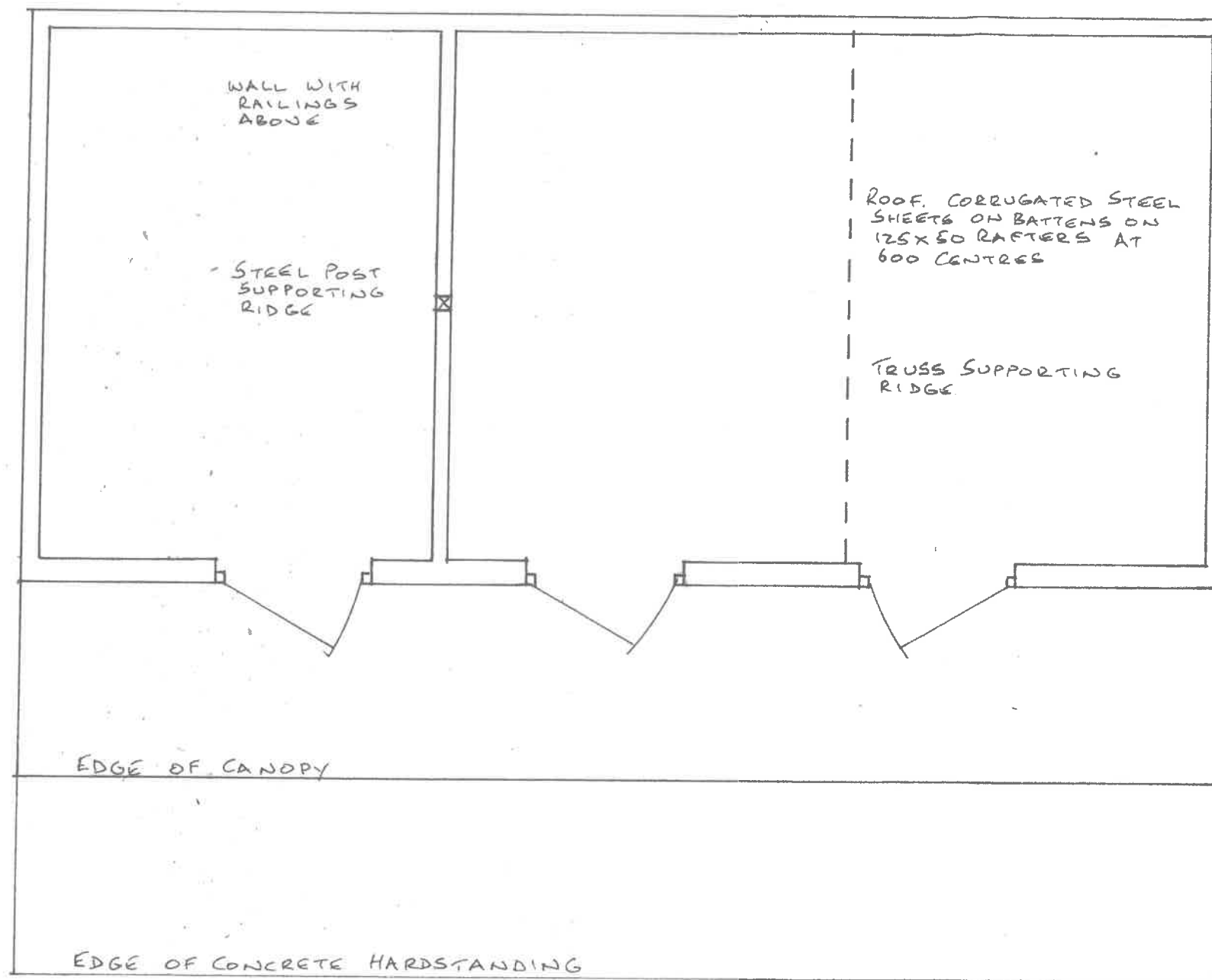
3. PROPOSED ELEVATIONS

25/10/16

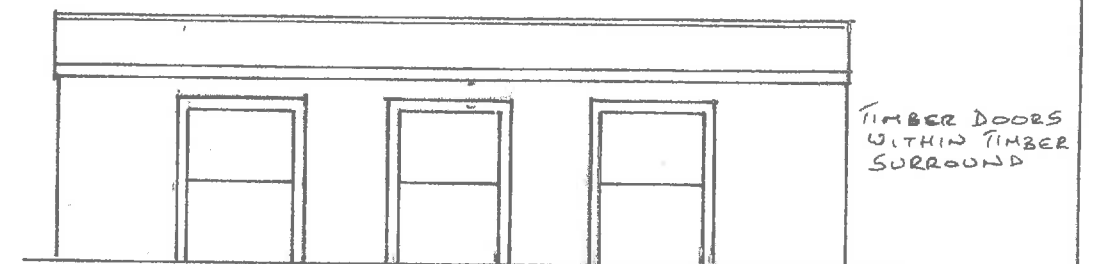
16/262/03

CONVERSION OF STABLE TO DWELLING

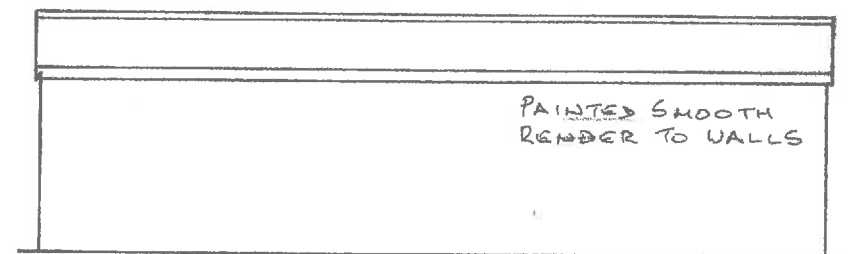
REAR 41, BURY OLD ROAD, AINSWORTH BL2 5PF



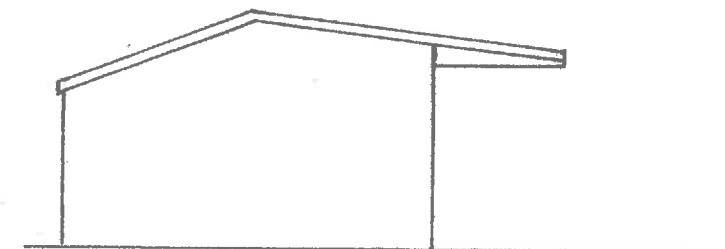
1 EXISTING PLAN



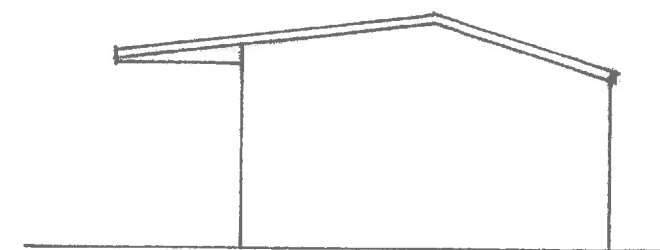
FRONT ELEVATION



REAR



SIDE



SIDE

SCALE 1:50 & 1:100 @ A3.

25/10/16

16/262/01

CONVERSION OF STABLE TO DWELLING
REAR 41, BURY OLD ROAD, AINSWORTH BL2 5PF

Ward: Radcliffe - West

Item 08

Applicant: Mr I MacDonald

Location: Land at Bolton Street/Holly Bank Street, Radcliffe, Manchester, M26 3SS

Proposal: Erection of 9 no. garages

Application Ref: 60810/Full

Target Date: 09/01/2017

Recommendation: Approve with Conditions

Description

The application relates to a strip of land located on the corner of the junction of Bolton Street and Hollybank Street. There are a mixture of uses within the immediate vicinity of the site, with residential properties located to the north and west, and commercial premises located to the south and east. The site has a dropped kerb that runs along Hollybank Street, demarcated parking bays and appears to be informally used as off street car parking for local businesses and residential properties.

Planning permission is sought for the construction of a row of 9 garages along the north west edge of the site. The garages would have a pitched roof and reach a maximum of approximately 3.6 metres. The garages have been set back from Hollybank Street to allow for a parking space of approximately 5 metres to be provided. The proposed garages will be constructed from brick, with profiled steel sheeting roofs and roller shutter doors to the front. The garages are intended for domestic parking purposes.

Relevant Planning History

60399 - Erection of 10 no. garages - Withdrawn by Applicant 24/08/2016 as one of the garages impeded visibility.

Publicity

30 Neighbour letters sent to the surrounding properties 16/11/2016.
Site Notice Posted 24/11/2016.

As a result of this publicity, 2 objections received from the director of FAM Engineering, Holly Bank Industrial Estate and an objector who did not provide an address with concerns regarding:

- Loss of the informal parking arrangement for the existing businesses creating more on street parking, which in turn could create access problems for the existing businesses.
- Increased traffic worsening the existing bottleneck at the road.
- Possibility of the garages being used as commercial properties for the storage of goods and vehicle repairs.
- Current problem with fly-tipping at site has not been addressed by the current owner.
- No demand from local residents for garages.
- Applicant has not taken the opportunity to address objections raised during previous application that was subsequently withdrawn.
- Applicant should demonstrate that they will ensure that the garages will only be used for residential use, and that they can properly manage this.

Consultations

Traffic Section - No objection subject to conditions requiring the implementation of a 1 metre knee rail, visibility splay, minimum hardstanding lengths of 5 metres and roller shutter doors being provided.

Environmental Health Contaminated Land - No objection subject to condition relating to unforeseen contamination during construction.

Unitary Development Plan and Policies

H5/1	Area Improvement
EN1/2	Townscape and Built Design
SPD6	Supplementary Planning Document 6: Alterations & Extensions
HT6/2	Pedestrian/Vehicular Conflict
H2/3	Extensions and Alterations
EN1/1	Visual Amenity

Issues and Analysis

The following report includes analysis of the merits of the application against the relevant policies of both the National Planning Policy Framework (NPPF) and the adopted Bury Unitary Development Plan (UDP) together with other relevant material planning considerations. The policies of the UDP that have been used to assess this application are considered to be in accordance with the NPPF and as such are material planning considerations. For simplicity, just the UDP Policy will be referred to in the report, unless there is a particular matter to highlight arising from the NPPF where it would otherwise be specifically mentioned.

Policies

The site is unallocated in the UDP and as such this scheme should be judged on the basis of its own merits.

EN1/2 - Townscape and Built Design ensures that the council will give favourable consideration to proposals which do not have an unacceptable adverse effect on the particular character and townscape of the Borough.

HT6/2 - Pedestrian/Vehicular Conflict seeks to ensure that the Council takes action as appropriate to reduce pedestrian/vehicular conflict.

Layout

The proposed garages would run along the north west side of the site along the boundary with no. 14 Bolton Street. The proposed garages are simple in design, single storey, with the use of brickwork and steel roof sheeting replicating the materials used in the neighbouring residential and commercial buildings. The proposed garages are of an appropriate height, scale and form when viewed in connection with the existing street scene.

As such, the proposed garages are considered to be acceptable and comply with UDP Policy EN1/2.

Residential Amenity

There are no adopted aspect standards between residential and non-residential uses. However, Supplementary Planning Document 6 provides guidance on aspect standards between residential properties and as such, would be a reasonable guide in this case.

The proposed garage colony would be located along the side boundary of No. 14 Bolton Street which is the only residential property to have a direct relationship with the proposal. No. 14 has 2 first floor, and 2 ground floor windows that face the rear of the proposed garages. However these windows appear to relate to non-habitable rooms and are located on the side elevation of the dwelling, rather than the principal elevation and as such are not afforded the same protection as habitable room windows.

The proposed garages project approximately 12.2 metres beyond the front elevation and 11.7 metres beyond the rear elevation of No. 14. It is noted that due to this projection the proposed garages would impact on a 45 degree line as taken from the mid-point of the eaves

of the adjacent principal windows of No. 14. Due to this the garages have been assessed against a 25 degree line on the rising plane from the mid point of these windows to ensure that they would not have a detrimental impact on the neighbouring properties right to daylight and sunlight. As the proposed garages are located on a site that is slightly lower than the dwelling at No. 14 the top of the wall does not intercept this 25 degree line. In this instance therefore it is considered that the proposal would not lead to unacceptable levels of overshadowing or impact on outlook.

As such, it is considered that the proposal complies with the guidance contained within SPD 6 and would not have an adverse impact on the residential amenity of No. 14.

Highways

The Traffic Section have been consulted as part of this application and have raised no objection to the proposal in terms of highway safety providing adequate conditions are attached to the grant of any planning permission.

The proposed plan shows a 1 metre high timber knee rail adjacent to Bolton Street ensuring that vehicles can only enter and exit the site via the existing dropped kerb that runs along Hollybank Street. The proposed plans have plotted a visibility splay and this shall be implemented to ensure the inter visibility of the users of the site and the adjacent highways in the interests of road safety.

In this instance therefore it is considered that the proposal complies with Policy HT6/2 and would maintain highway safety for both pedestrian and vehicular users.

There are no adopted driveway length for non-residential alterations however, Supplementary Planning Document 6 provides guidance and as such, would be a reasonable guide in this case.

All proposals should ensure that there is sufficient space to accommodate a vehicle safely within the curtilage of the site, to prevent overhang of the adopted highway. SPD 6 requires a driveway of 5.5 metres for garages with up and over doors, and 5 metres in most other cases. In this instance the proposal includes roller shutter doors as opposed to up and over garage doors and as such a 5 metre driveway is required. Driveway lengths of approximately 5030 mm have been provided to the front of each garage and as such is considered to be acceptable. A condition will be attached to ensure that roller shutter doors are implemented as per the submitted plan.

Neighbour Objections

Objections have been raised with regards to access and loss of parking provision. The proposal has been assessed by the Traffic Section and they have raised no objection to the proposal in terms of highways safety. Whilst the current business units and visitors may use this site for parking this appears to be via an informal arrangement. The site is not formally associated with any particular unit or business and the applicant states within their Design and Access Statement that the site is not available for use by the public. In this instance therefore the use of the land as a car park is a private legal matter between the land owner and the adjacent business and not a material planning consideration.

The previous application was withdrawn in order to address concerns raised by the Traffic Section. These concerns were addressed within the resubmitted application. It is considered that objections raised previously have been addressed within this report.

The use of the garages would be restricted by condition for residential parking only, rather than commercial activity. The need or demand for this type of development is not a material planning consideration and as such is not relevant to the decision.

As above matters controlled by other non-planning legislation are non material planning considerations. The existing fly-tipping problem at the site appears to have been reported to the Council's Waste Management Services and as such is not relevant to the decision.

Statement in accordance with Article 35(2) Town and Country Planning (Development Management Procedure) (England) (Amendment) Order 2015

The proposal complies with the development plan and would improve the economic, social and environmental conditions of the area. It therefore comprises sustainable development and the Local Planning Authority worked proactively and positively to issue the decision without delay. The Local Planning Authority has therefore implemented the requirement in Paragraphs 186-187 of the National Planning Policy Framework.

Recommendation: Approve with Conditions

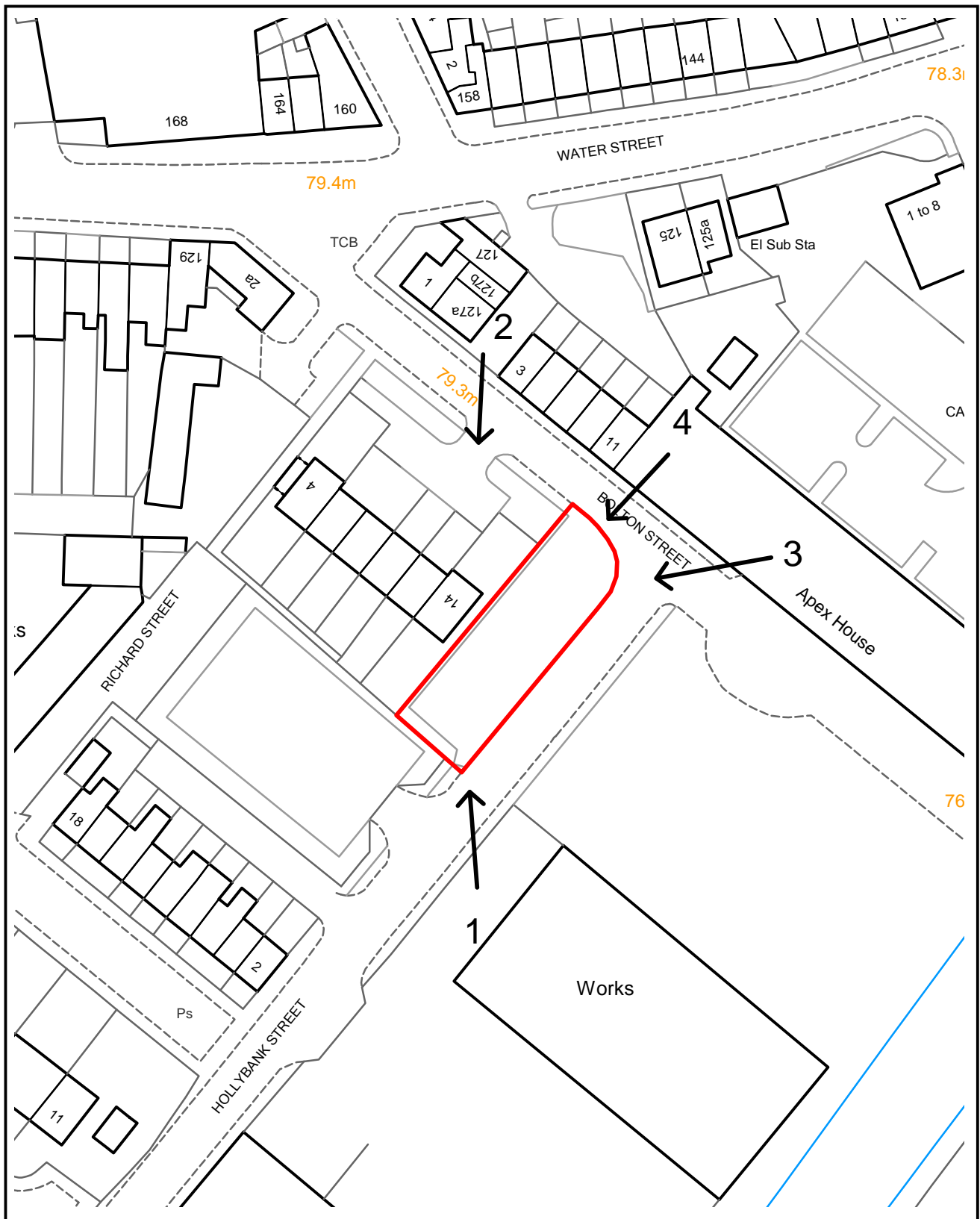
Conditions/ Reasons

1. The development must be begun not later than three years beginning with the date of this permission.
Reason. Required to be imposed by Section 91 Town & Country Planning Act 1990.
2. This decision relates to drawings numbered 1078-06 and 1078-10 and the development shall not be carried out except in accordance with the drawings hereby approved.
Reason. For the avoidance of doubt and to ensure a satisfactory standard of design pursuant to the policies of the Bury Unitary Development Plan listed.
3. Details of the materials to be used in the external elevations, together with details of their manufacturer, type/colour and size, shall be submitted to and approved in writing by the Local Planning Authority before the development is commenced. Only the approved materials shall be used for the construction of the development.
Reason. No material samples have been submitted and are required in the interests of visual amenity and to ensure a satisfactory development pursuant to UDP Policy EN1/1 Visual Amenity.
4. Where during any works on site, unforeseen contamination is suspected or found, or contamination is caused, works on the site shall cease and the Local Planning Authority shall be notified immediately. The developer shall then produce a risk assessment and submit remediation proposals, if required, for approval to the Local Planning Authority. On approval of the remediation strategy, the development shall then be carried out in accordance with the approved details and process including any required timescales.
Reason. To secure the satisfactory development of the site in terms of human health and the wider environment and pursuant to National Planning Policy Framework Section 11 - Conserving and enhancing the natural environment.
5. The development hereby approved shall not be brought into use unless and until the 1.0m high timber knee railing located clear of the visibility splay indicated on approved plan reference 1078-10, the resurfacing of the narrow adopted rubbing strip abutting Holly Bank Street and any associated kerb remedial works required following construction of the proposed garages have been implemented with the approved boundary treatment thereafter maintained.
Reason. To secure the satisfactory development of the site in terms of highway safety, ensure good highway design and maintain the integrity of the adopted highway, in the interests of highway safety pursuant of Policy HT6/2 - Pedestrian/Vehicular Conflict.

6. The visibility splay indicated on approved plan 1078-10 shall be implemented to the written satisfaction of the Local Planning Authority before the development is brought into use and subsequently maintained free of obstruction above the height of 0.6m on land within the applicant's control.
Reason. To ensure the intervisibility of the users of the site and the adjacent highways in the interests of road safety pursuant of Policy HT6/2 - Pedestrian/Vehicular Conflict.
7. The garages shall be fitted with roller shutter doors or a similar approved type which do not project outwards at any time during or after operation and shall thereafter be maintained and minimum hardstanding lengths of 5.0m clear of the adopted highway shall be provided at each garage and thereafter maintained.
Reason. To enable a vehicle to stand clear of the highway whilst the garage doors are opened and to allow adequate space to maintain a vehicle clear of the highway in the interests of road safety pursuant to Policy H2/3 - Extensions and Alterations of the Bury Unitary Development Plan and associated Supplementary Planning Document 6 - Alterations and Extensions to Residential Properties.
8. The garages hereby approved shall be used solely for domestic purposes only and shall not be used in connection with any trade or business.
Reason. The garages are located close to a residential property and the garages use for domestic purposes only would maintain appropriate levels of residential amenity.

For further information on the application please contact **Helen Goldsbrough** on **0161 253 5277**

Viewpoints



PLANNING APPLICATION LOCATION PLAN

APP. NO 60810

**ADDRESS: Land at Bolton/Holly Bank Street
Radcliffe**

Planning, Environmental and Regulatory Services

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Bury
COUNCIL

60810

Photo 1



Photo 2



Photo 3



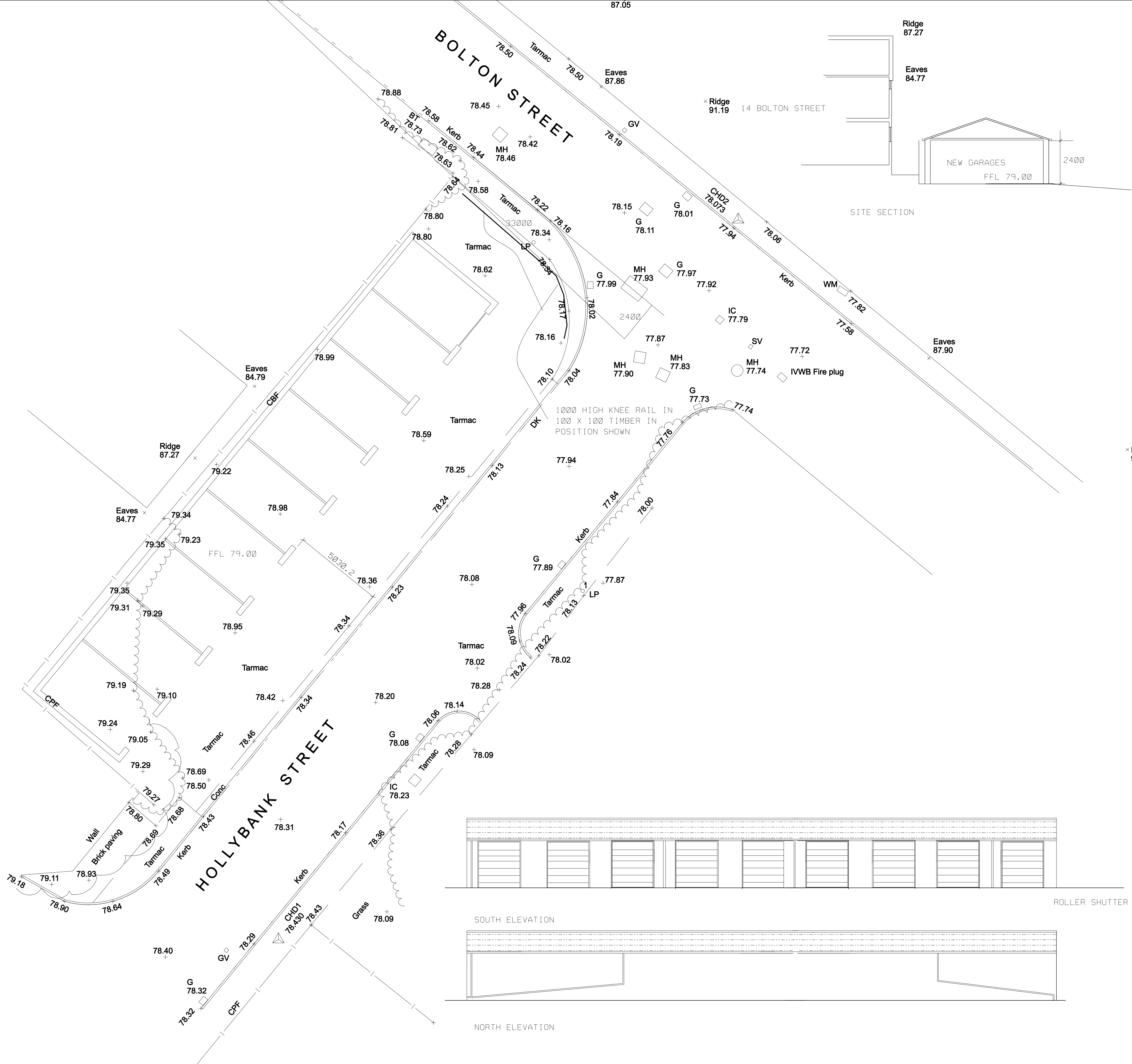
Photo 4



ALL DIMENSIONS TO BE VERIFIED
ON SITE AND THE
ARCHITECT INFORMED
OF ANY DISCREPANCY.

REVISION

DATE



SITE SECTION

EAST ELEVATION

WEST ELEVATION

SOUTH ELEVATION

NORTH ELEVATION

PILGRIM Associates

UNIT 5, NILE MILL A,
FIELDS NEW ROAD
CHADDERTON,
OLDHAM,
GREATER MANCHESTER
OL9 6NH
TEL & FAX: 0161 627 1536

CHARTERED ARCHITECTS
CHARTERED SURVEYORS

DRAWN DATE SCALE
DECEMBER 2016 1.100 AT A1

PROPOSED GARAGES
BOLTON STREET
RADCLIFFE

DRAWING NO.
1078-10

Ward: Prestwich - St Mary's

Item 09

Applicant: Mrs Lisa Dalgarno

Location: 7 Fairfax Road, Prestwich, Manchester, M25 1AS

Proposal: Change of use from Citizens Advice Bureau (Class A2) to wine bar (Class A4) and alterations to front/rear elevations

Application Ref: 60836/Full

Target Date: 16/01/2017

Recommendation: Approve with Conditions

Description

The application relates to a two storey red brick terraced property in the middle of a row of similar properties. The ground floor has a floor area of approximately 65sqm. The attached property at No.5 is a family mediation centre and No.9, on the other side, are solicitors offices. There is a pedestrian ramped access at the front and vehicular access at the rear with two parking spaces. The property, last used as a Citizens Advice Bureau, is currently vacant.

It is proposed to change the use of the premises to a wine bar. The ground floor would comprise a bar area (approx 40sqm) and a small kitchen and services. The first floor would have an additional bar area and a store and there would be a small ancillary office in the roofspace. External alterations to front comprise a new window configuration in the square bay which would also be reroofed in slate.

Opening hours are indicated as 11am to 11pm Sunday to Thursday and to midnight on Fridays and Saturdays. The number of employees is not indicated.

Relevant Planning History

None relevant.

Publicity

The following 52 neighbours were notified by letter dated 23/11/16. No.3-11(odd) Fairfax Road, 472 and 494 Bury New Road and flats at The Radius. Objections received from the tenant (Children First Family Mediation) and owner of 5 Fairfax Road. Objections are summarised:

- A wine bar would have a detrimental impact on the confidentiality and discretion that clients visiting the mediation facility expect.
- It would be 'unpleasant and unacceptable' to have children walking past people drinking in the evenings and at weekends.
- Concerns about noise intruding on the atmosphere/operation of the mediation centre which needs to be calm.
- Deliveries may block access and parking at the rear is limited.

The objectors have been notified of the Planning Control Committee meeting.

Consultations

Environmental Health - No objection.

Greater Manchester Police - No objection.

Traffic Section - No objection.

Unitary Development Plan and Policies

EN1/2 Townscape and Built Design

EN1/8 Shop Fronts

S2/6	Food and Drink
HT2/4	Car Parking and New Development
HT5/1	Access For Those with Special Needs

Issues and Analysis

The following report includes analysis of the merits of the application against the relevant policies of both the National Planning Policy Framework (NPPF) and the adopted Bury Unitary Development Plan (UDP) together with other relevant material planning considerations. The policies of the UDP that have been used to assess this application are considered to be in accordance with the NPPF and as such are material planning considerations. For simplicity, just the UDP Policy will be referred to in the report, unless there is a particular matter to highlight arising from the NPPF where it would otherwise be specifically mentioned.

Use - The site is within Prestwich Town Centre and in an area characterised by a mix of commercial and community uses.

Given that the premises are modest in size, not within a prime shopping frontage and has been vacant for at least 18 months, it is considered that in allowing an existing vacant property to be brought back into active use, the proposal would enhance the viability and vitality of the town centre. As such, the proposal, in terms of use, is acceptable and complies with UDP town centre policies.

Visual amenity - The existing ground floor windows in the square bay would be replaced to form a single window split into three sections to maintain the vertical emphasis of the frontage. The bay would be rendered and the existing concrete roof tiles replaced with natural slate. It is considered that the proposal, in bringing the premises back into active use, would improve the character and visual amenity along this part of the centre. As such the proposal is acceptable in terms of visual amenity and the character of the town centre and complies with UDP Policies EN1/2 relating to streetscape.

Residential Amenity - Given that the site is within the town centre and is relatively modest in scale. The small wine bar is not likely to generate excessive noise and disturbance or have a seriously adverse impact on nearby residents within the Radius across Fairfax Road. The proposed opening hours to 11pm Sunday to Thursday and midnight on Fridays and Saturdays are not excessive for a town centre use. The Late Victorian properties along this row are likely to have adequate noise attenuation properties along the party walls and particularly between businesses, it is unlikely that noise insulation would be required. Notwithstanding this however, a condition would be added to any approval requiring a survey to be carried out to assess the state of the existing party walls to determine whether noise attenuation would actually be required.

The proposal is considered to be acceptable in terms of residential amenity and complies with UDP Policy S2/6 Food and Drink.

Access - Access is as existing and relatively level from the footway on Bolton Street and therefore would comply with UDP Policy HT5/1.

Traffic and Parking - The site is in the town centre and in a very sustainable location with good links to public transport and public car parks nearby. Given the size of the premises, the traffic generated would be negligible within the town centre as a whole. There are tow parking spaces at the rear for staff and the property would be service from the rear. Whilst the space at the rear for larger service/delivery vehicles is limited, it is likely that suitable arrangements could be made.

The Traffic Section has raised no objection to the proposal which in terms of traffic and parking would be acceptable and comply with UDP Policy S2/6 Food and Drink.

Objections - The concerns of the family liaison centre next door with regard to privacy and

confidentiality would not constitute strong planning reasons to refuse the application. The centre is on a busy commercial road opposite a restaurant and the privacy issues raised would not be considered significant particularly as the wine bar could not have an external seating area at the front due to the presence of the access ramp. There are views out of the front window onto Fairfax Road but the bay at the front does not have a window directly facing the family liaison centre next door. The concerns about noise and disturbance are addressed by the section on 'Residential Amenity'. The concerns about traffic and parking have also been addressed above.

Statement in accordance with Article 35(2) Town and Country Planning (Development Management Procedure) (England) (Amendment) Order 2015

The Local Planning Authority worked positively and proactively with the applicant to identify various solutions during the application process to ensure that the proposal comprised sustainable development and would improve the economic, social and environmental conditions of the area and would accord with the development plan. These were incorporated into the scheme and/or have been secured by planning condition. The Local Planning Authority has therefore implemented the requirement in Paragraphs 186-187 of the National Planning Policy Framework.

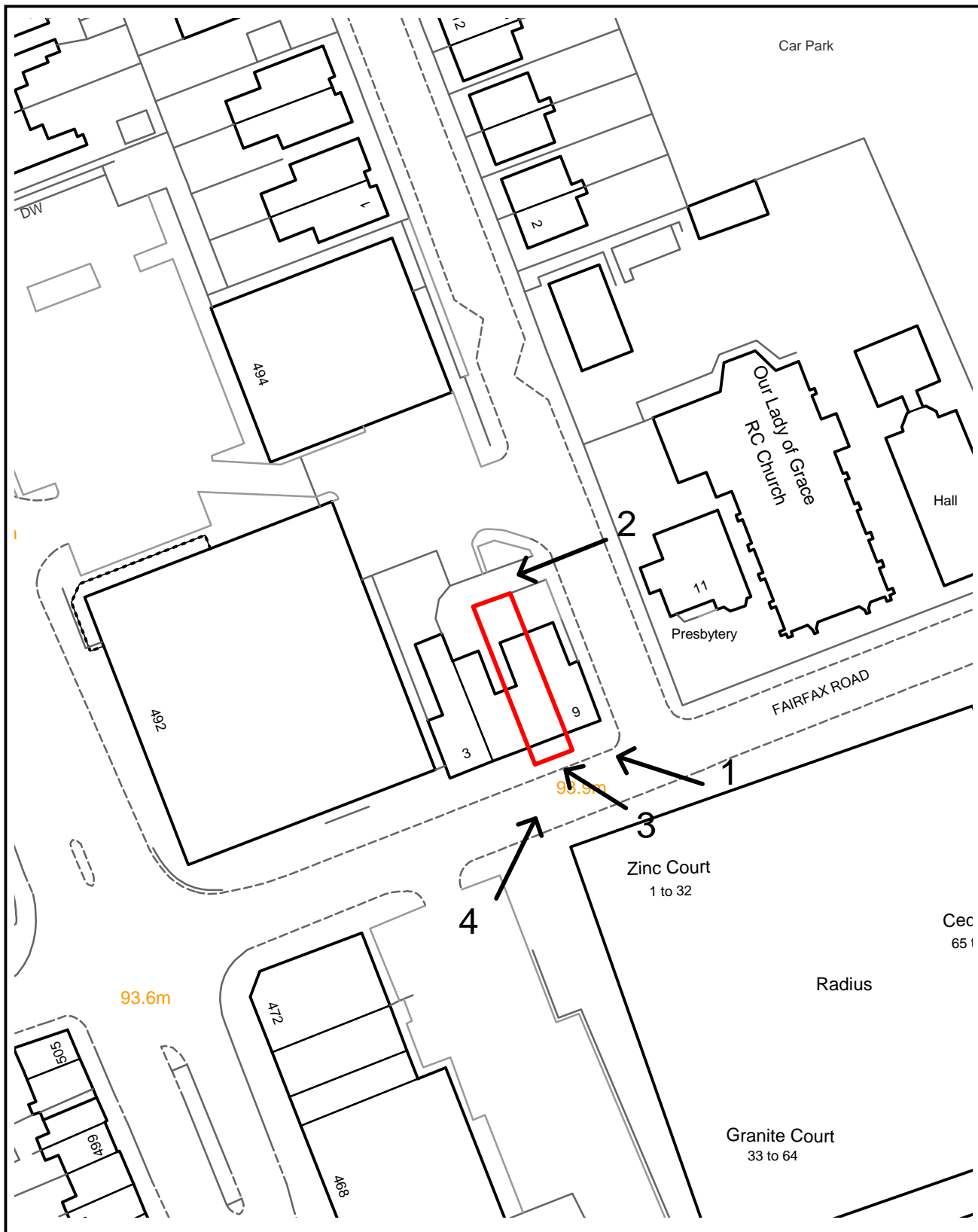
Recommendation: Approve with Conditions

Conditions/ Reasons

1. The development must be begun not later than three years beginning with the date of this permission.
Reason. Required to be imposed by Section 91 Town & Country Planning Act 1990.
2. This decision relates to drawings numbered LU054-P01, P02, P03/A and P04 and the development shall not be carried out except in accordance with the drawings hereby approved.
Reason. For the avoidance of doubt and to ensure a satisfactory standard of design pursuant to the policies of the Bury Unitary Development Plan listed.
3. Before the Class A4 use hereby approved commences, a noise survey of the premises from a suitable qualified person, including any appropriate acoustic insulation/treatment required, shall be submitted to and approved in writing by the Local Planning Authority. The acoustic treatment shall be implemented in full before the use commences.
the premises shall be acoustically insulated and treated to limit the break out of noise in accordance with
Reason. To safeguard the amenities of the occupiers of neighbouring properties pursuant to UDP Policy EN7/2 Noise Pollution.
4. The A4 use hereby permitted shall not be open to customers outside the following times:
1000hrs - 2300hrs Sunday to Thursday and Bank Holidays
1000hrs - 0000hrs (Midnight) Fridays and Saturdays
Reason. To safeguard the amenity of neighbours pursuant to UDP Policy and S2/6 Food and Drink.

For further information on the application please contact **Tom Beirne** on **0161 253 5361**

Viewpoints



PLANNING APPLICATION LOCATION PLAN

APP. NO 60836

**ADDRESS: 7 Fairfax Road
Prestwich**



Bury
COUNCIL

Planning, Environmental and Regulatory Services

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60836

Photo 1



Photo 2



Photo 3



Photo 4



Revisions

revision	date	comments
A	22.12.16	Frontage amended following P.O. comments

Job Title

Change of Use from A2 to A4 and changes to frontage

Site Address

7 Fairfax Rd, Prestwich, M25 1AS

Drawing Title

Existing & proposed elevations

Scale

1:100 @ A3

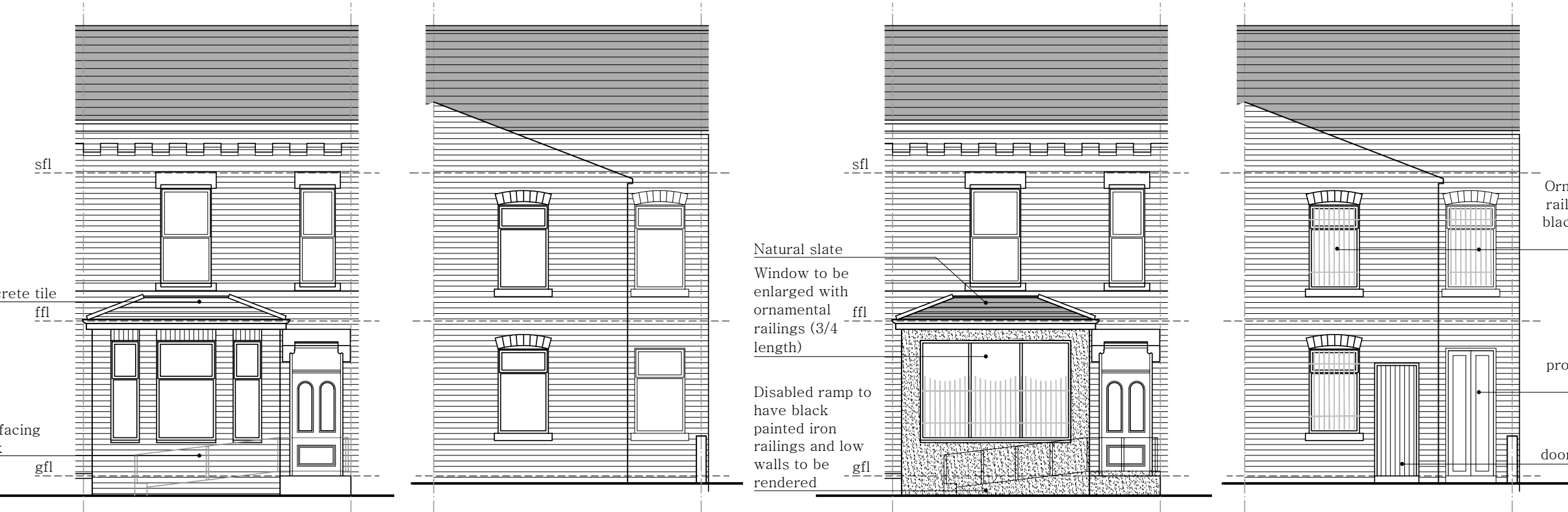
Date

07.11.16

Drawn

uf

A. Unit 4
Springbank Buildings
226 - 248 Every St
Nelson
BB9 7BS
T. 07894401574
E. info@lumitekton.com



Existing elevation (front)

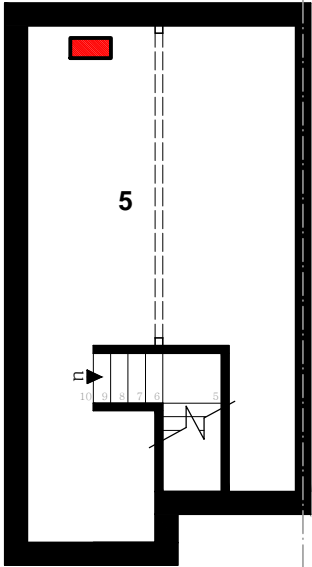
Existing elevation (rear)

Proposed elevation (front)

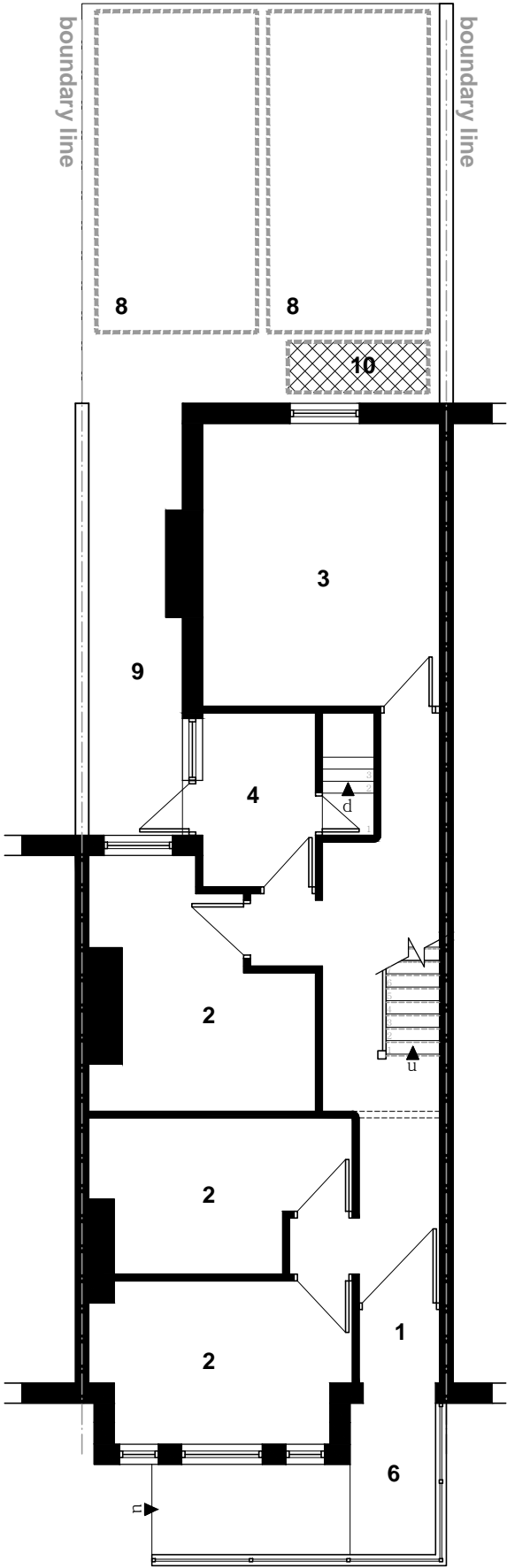
Proposed elevation (rear)

Key

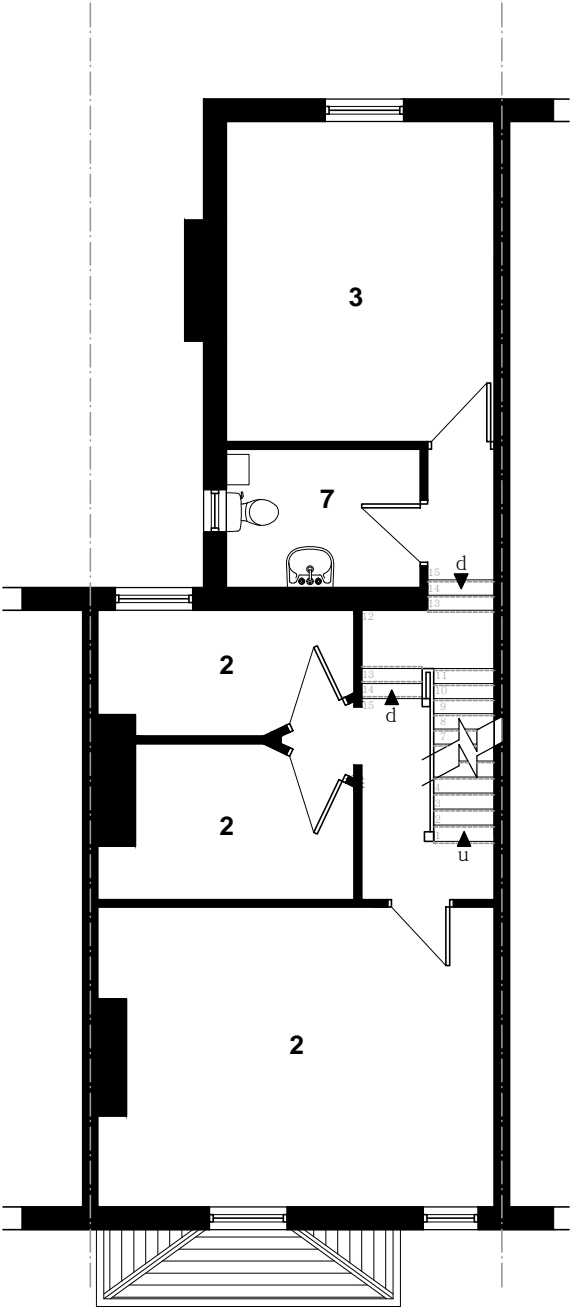
- Mains Gas
1. Entrance
2. Office
3. Meeting room
4. Kitchen
5. Cellar
6. Disabled ramp
7. WC
8. Car parking space
9. Yard
10. Bin store



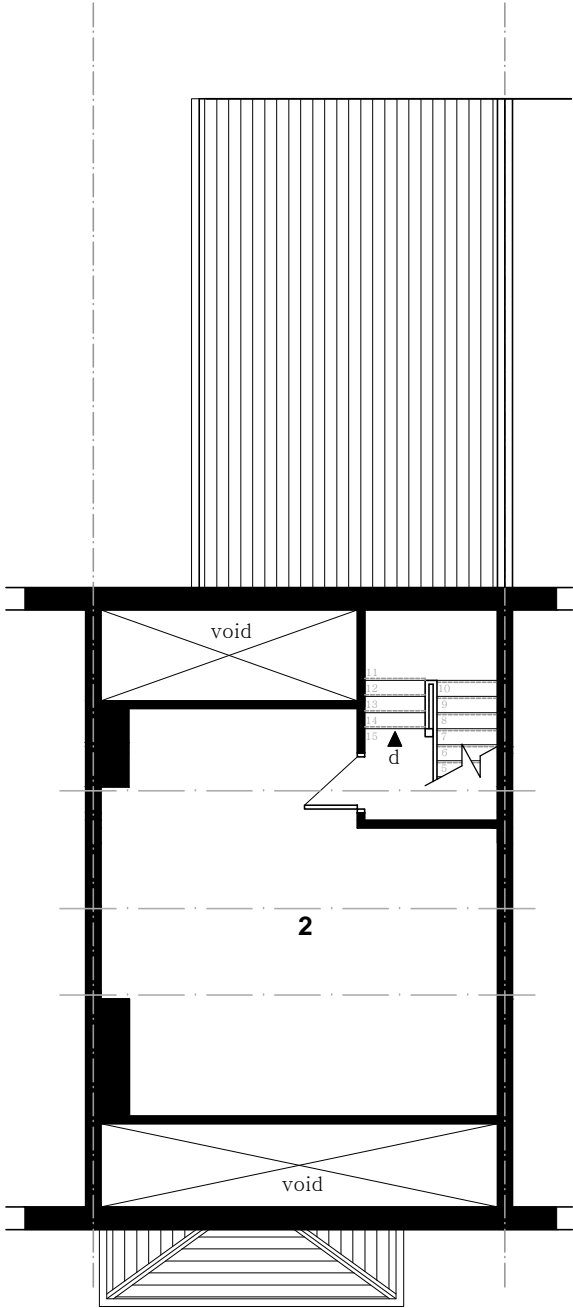
Existing basement plan



Existing ground floor plan



Existing first floor plan



Existing second floor plan

LUM
LUMITEKTON

LU054-P02

Revisions

revision	date	comments
-	-	-

Job Title

Change of Use from A2 to A4 and changes to frontage

Site Address

7 Fairfax Rd, Prestwich, M25 1AS

Drawing Title

Existing plans

Scale

1:100 @ A3

Date

07.11.16

Drawn

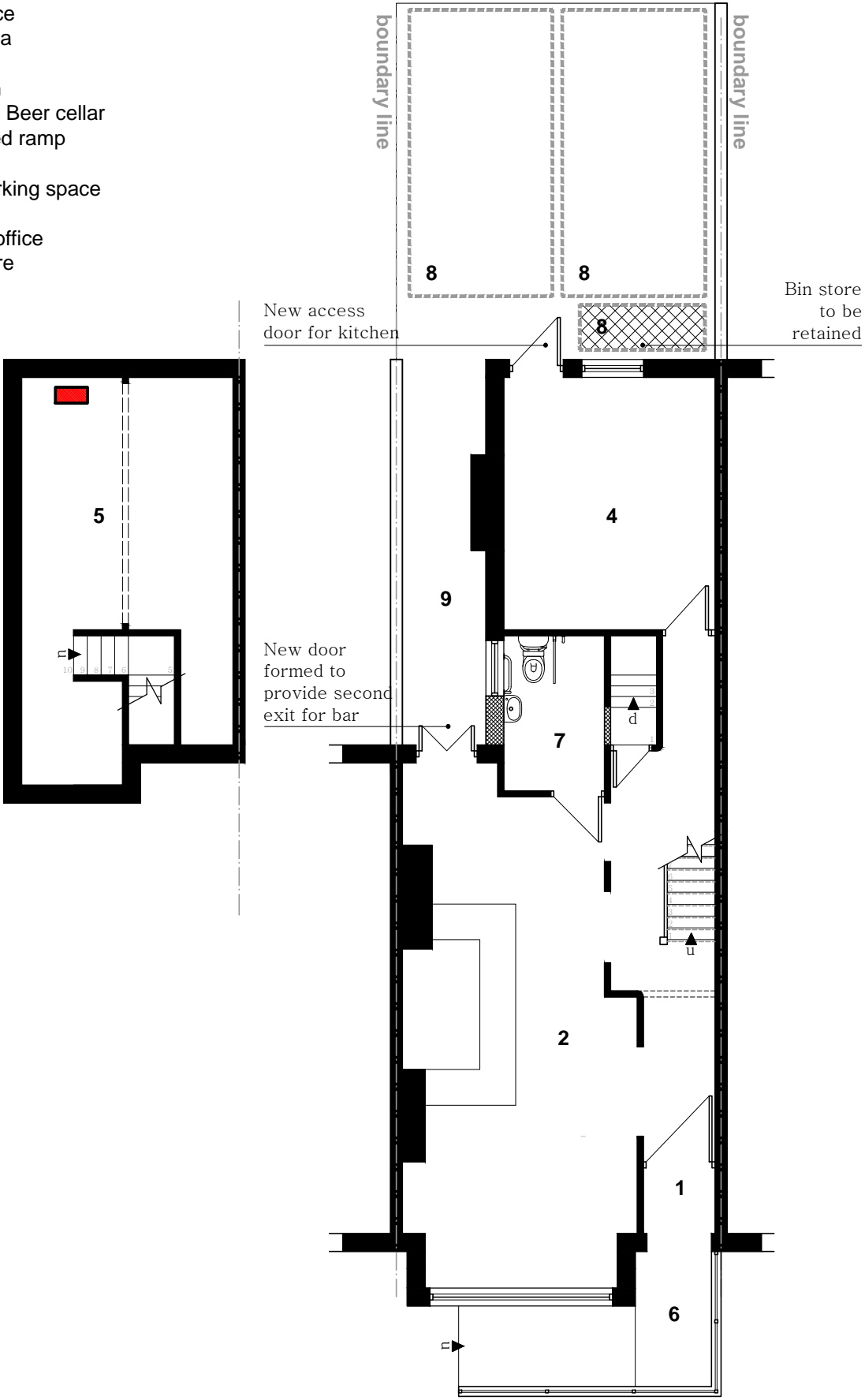
uf

A. Unit 4
Springbank Buildings
226 - 248 Every St
Nelson
BB9 7BS
T. 07894401574
E. info@lumitekton.com

Key

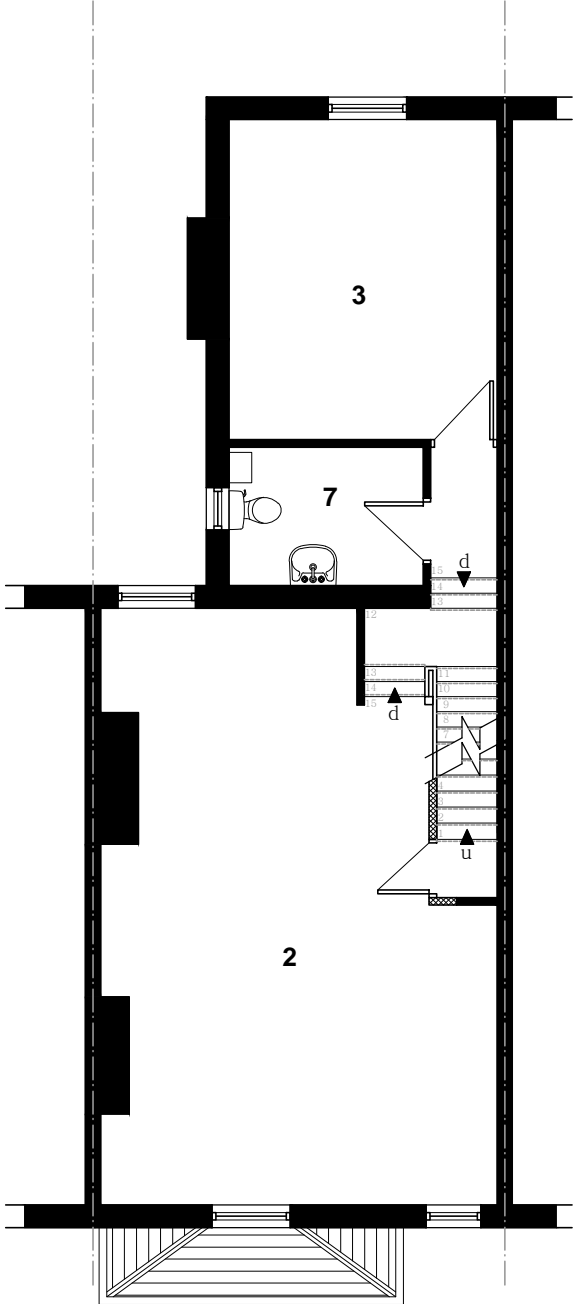
Mains Gas

- 1. Entrance
- 2. Bar area
- 3. Store
- 4. Kitchen
- 5. Wine & Beer cellar
- 6. Disabled ramp
- 7. WC
- 8. Car parking space
- 9. Yard
- 10. Store/ office
- 11. Bin store

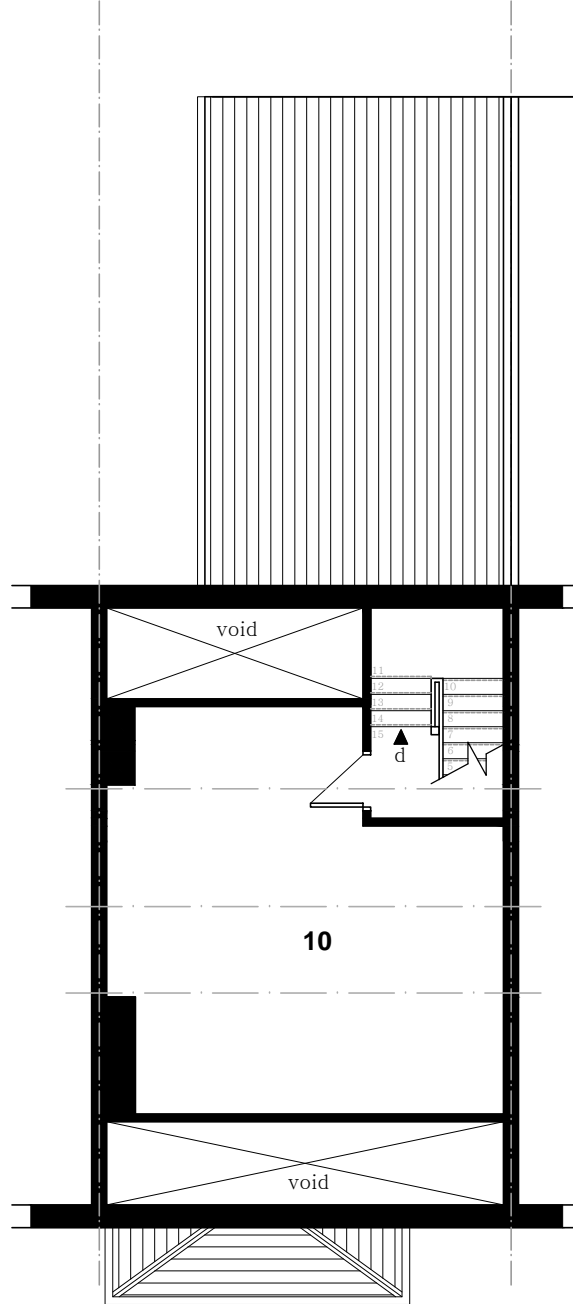


Proposed basement plan

Proposed ground floor plan



Proposed first floor plan



Proposed second floor plan

LUM
LUMITEKTON

LU054-P04

Revisions

revision	date	comments
-	-	-

Job Title

Change of Use from A2 to A4 and changes to frontage

Site Address

7 Fairfax Rd, Prestwich, M25 1AS

Drawing Title

Proposed plans

Scale

1:100 @ A3

Date

07.11.16

Drawn

uf

A. Unit 4
Springbank Buildings
226 - 248 Every St
Nelson
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E. info@lumitekton.com

Ward: Ramsbottom and Tottington -
Ramsbottom

Item 10

Applicant: Mr John Russell

Location: Land adjacent to 12 Est Bank Road, Ramsbottom, Bury, BL0 9RA

Proposal: Change of use of land for the extension of residential curtilage and erection of boundary fencing

Application Ref: 60878/Full

Target Date: 02/02/2017

Recommendation: Approve with Conditions

Description

The application relates to a two storey semi-detached house on the corner of Est Bank Road as it bends around past the entrance to Woodhey High School, situated to the east. The area is residential in character.

The proposed fence has already been erected to enclose the extended side garden area of 12 Est Bank Road - see photographs. The fence is 2m high and comprises concrete posts and panels on the lower section with timber panel fencing above.

The new boundary fence extends off the original boundary fence at the front corner and runs back 13m to join up with the neighbours fence at the rear (361 Bolton Road West and subject to a similar proposal being considered on this Planning Committee agenda - 60879). The section of land enclosed is approximately 5m wide.

Relevant Planning History

16/0397 - Enforcement - Change of use of land to garden - Planning application received.
60878 - Change of use of land to garden and erection of Boundary Fence - undecided.

Publicity

The following neighbours were notified by letter dated 06/12/16. Nos. 357 - 361 Bolton Road West, 8 and 10 Est Bank and Woodhey High School.

One letter received from Woodhey High School. Concerns are summarised:

- Reduction in width of access/egress to school for pupils and visitors.
- Restrictions in visibility for drivers/ pedestrians turning onto Bolton Road.
- Concern about proximity of trees to the new fence.

The school has been notified of the Planning Control Committee meeting.

Consultations

Traffic Section - No comment to date.

Unitary Development Plan and Policies

H2/3 Extensions and Alterations

EN1/2 Townscape and Built Design

SPD6 Supplementary Planning Document 6: Alterations & Extensions

Issues and Analysis

The following report includes analysis of the merits of the application against the relevant policies of both the National Planning Policy Framework (NPPF) and the adopted Bury Unitary Development Plan (UDP) together with other relevant material planning considerations. The policies of the UDP that have been used to assess this application are considered to be in accordance with the NPPF and as such are material planning considerations. For simplicity, just the UDP Policy will be referred to in the report, unless there is a particular matter to highlight arising from the NPPF where it would otherwise be specifically mentioned.

Use - Whilst extending the side garden area of the house at the end of the row of houses has reduced the width of the existing grassed verge, there is a width of over 6m remaining and this has maintained the relatively open feel of the area.

The site has no site specific allocations relating to it. However UDP Policy OL3/1 Protection of Urban Open Space states that within the urban area, development will not be permitted where it would lead to the loss of urban open space which provides:

- a) valuable visual amenity;
- b) valuable wildlife habitats, corridors and links;
- c) important outlets for recreation; or
- d) a buffer between incompatible uses or a link between other open land areas.

It is not considered that the area in question fulfills any of these criteria and therefore the proposal is acceptable in principle.

Visual amenity - The new boundary fence would, with a concrete base and timber fence panels above to a maximum height of 2m, be in keeping with the and would not be incongruous within the streetscape. Complies with UDP Policies EN1/2 and H2/3.

Residential amenity - No issues arising.

Trees - The small trees on the adjacent grassed land are not affected by the proposal.

Traffic/ highway safety - The wall/fence is sufficiently set back from the back edge of the footway so as not to cause any serious highway safety issues. The proposal is acceptable in terms of highway safety and complies with UDP Policy H2/3.

Objection - The concerns of the high school are not considered to be valid reasons to refuse the application. Although the width of the grassed area, which should not really be walked upon anyway, is being narrowed, the actual public footway would remain unaffected. Visibility around the corner of the site would not be affected.

Statement in accordance with Article 35(2) Town and Country Planning (Development Management Procedure) (England) (Amendment) Order 2015

The proposal complies with the development plan and would improve the economic, social and environmental conditions of the area. It therefore comprises sustainable development and the Local Planning Authority worked proactively and positively to issue the decision without delay. The Local Planning Authority has therefore implemented the requirement in Paragraphs 186-187 of the National Planning Policy Framework.

Recommendation: Approve with Conditions

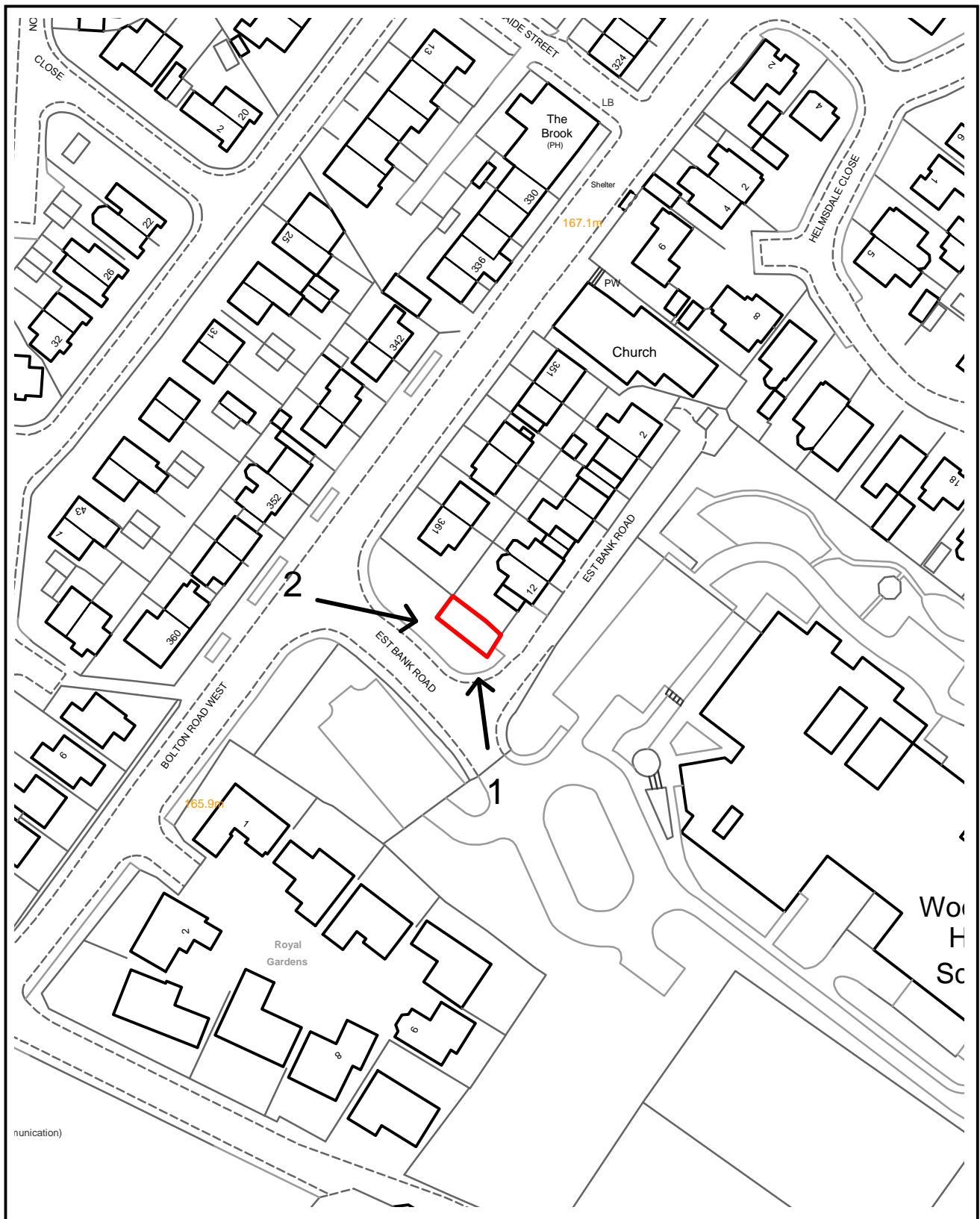
Conditions/ Reasons

1. This decision relates to drawings numbered 2016/118-01A and the development

shall not be carried out except in accordance with the drawings hereby approved.
Reason. For the avoidance of doubt and to ensure a satisfactory standard of design pursuant to the policies of the Bury Unitary Development Plan listed.

For further information on the application please contact **Tom Beirne** on **0161 253 5361**

Viewpoints



PLANNING APPLICATION LOCATION PLAN

APP. NO 60878

**ADDRESS: Land Adjacent To 12 Est Bank Road
Ramsbottom**

Planning, Environmental and Regulatory Services

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Bury
COUNCIL

60878

Photo 1



Photo 2





IMAGE INDICATING FENCE THAT HAS BEEN ERECTED TO DEFINE THE INCREASE IN RESIDENTIAL CURTLAGE



IMAGE OF GRASS ONE TO ACCESS ROAD WITH NEW FENCING INDICATED



Client			
John Russell			
Project			
Proposed extension of residential curtilage to land next to 12 Est Bank Rd Ramsbottom, Bury BL0 9QZ			
Job No.	Area	File Ref	
2016/118			
Title			
Site Plan			
2016/118-01			rev. A
Date	Drn.	Scale	Checked
26.10.16	B. Edmondson	1:200@A1	

eds
edmondson
design services

20 Fernhill Drive
Stacksteads, Bacup
Lancashire
OL13 8JS
mob: 07528809176 tel 01706 870944
e:info@edmondsondesignservices.co.uk

Ward: Ramsbottom and Tottington -
Ramsbottom

Item 11

Applicant: Mr Vortre Williams

Location: Land adjacent to 361 Bolton Road West, Ramsbottom, Bury, BL0 9QZ

Proposal: Change of use of land for the extension of residential curtilage and erection of boundary fencing

Application Ref: 60879/Full

Target Date: 30/01/2017

Recommendation: Approve with Conditions

Description

The application relates to a two storey semi-detached house on the corner of Bolton Road and Est Bank Road. The area is predominantly residential in character with Woodhey High School situated to the east.

The proposed fence has already been erected to enclose the extended garden area - see photographs. Currently the fence is 2m high and comprises concrete posts and panels on the lower section with timber panel fencing above. The fence extends off the original corner point at the front and tapers back to run parallel with Est Bank Road. At the central point, the width of the land enclosed is about 4.2m. It is proposed to lower the front section of fencing, closest to Bolton Road West, to 1m in height to allow for improved visibility and reduce its impact on the streetscape at this corner.

It should be noted that a similar application, with a retrospective proposal to enclose a section of land adjoining the site, has been submitted by the occupiers of 12 Est Bank Road. This application (60878) is being considered at this Planning Control Committee meeting.

Relevant Planning History

60066 - Two storey extension at side; Raised decking at rear with steps to garden level - Approved 14/06/2016

60878 - Change of use of land to garden and erection of boundary fencing - undecided.

16/0398 - Enforcement - Change of use of land to garden - Planning application received.

Publicity

The following neighbours were notified by letter dated 06/12/16 - 346 - 360(even), 357 and 359 Bolton Road West, 8, 10 and 12 Est Bank and Woodhey High School.

One letter received from Woodhey High School. Concerns are summarised:

- Reduction in width of access/egress to school for pupils and visitors.
- Restrictions in visibility for drivers/ pedestrians turning onto Bolton Road.
- Concern about proximity of trees to the new fence.

The school has been notified of the Planning Control Committee meeting.

Consultations

Traffic Section - No objection.

Unitary Development Plan and Policies

EN1/2 Townscape and Built Design

H2/3 Extensions and Alterations

Issues and Analysis

The following report includes analysis of the merits of the application against the relevant policies of both the National Planning Policy Framework (NPPF) and the adopted Bury Unitary Development Plan (UDP) together with other relevant material planning considerations. The policies of the UDP that have been used to assess this application are considered to be in accordance with the NPPF and as such are material planning considerations. For simplicity, just the UDP Policy will be referred to in the report, unless there is a particular matter to highlight arising from the NPPF where it would otherwise be specifically mentioned.

Use - Whilst extending the side garden area of the house at the end of the row of houses has reduced the width of the existing grassed verge, there is a width of over 6m remaining and this has maintained the relatively open feel of the area.

The site has no site specific allocations relating to it. However UDP Policy OL3/1 Protection of Urban Open Space states that within the urban area, development will not be permitted where it would lead to the loss of urban open space which provides:

- a) valuable visual amenity;
- b) valuable wildlife habitats, corridors and links;
- c) important outlets for recreation; or
- d) a buffer between incompatible uses or a link between other open land areas.

It is not considered that the area in question fulfills any of these criteria and therefore the proposal is acceptable in principle.

Visual amenity - The new boundary fence would, with a height of 1m at the front and increasing to 2m to the rear, be in keeping with the general character of the area and would not be incongruous within the streetscape. Complies with UDP Policies EN1/2 and H2/3.

Residential amenity - No issues arising.

Traffic/ highway safety - The line of the fence has been tapered at the front corner to allow adequate visibility for vehicles exiting Est Bank Road and the height would be lowered to 1m along the section forward of the house itself. This height reduction would be required by an appropriate condition within a suitable time frame. The proposal is acceptable in terms of highway safety and complies with UDP Policy H2/3.

Trees - The small trees on the adjacent grassed land are not affected by the proposal.

Objection - The concerns of the high school are not considered to be valid reasons to refuse the application. Although the width of the grassed area, which should really be walked upon anyway, is being narrowed, the actual public footway would be unaffected. Visibility around the corner onto Bolton Road West would not be seriously affected given the boundary fence has been tapered in and the height would be reduced down to 1m forward of the house at No.361.

Statement in accordance with Article 35(2) Town and Country Planning (Development Management Procedure) (England) (Amendment) Order 2015

The proposal complies with the development plan and would improve the economic, social and environmental conditions of the area. It therefore comprises sustainable development and the Local Planning Authority worked proactively and positively to issue the decision without delay. The Local Planning Authority has therefore implemented the requirement in Paragraphs 186-187 of the National Planning Policy Framework.

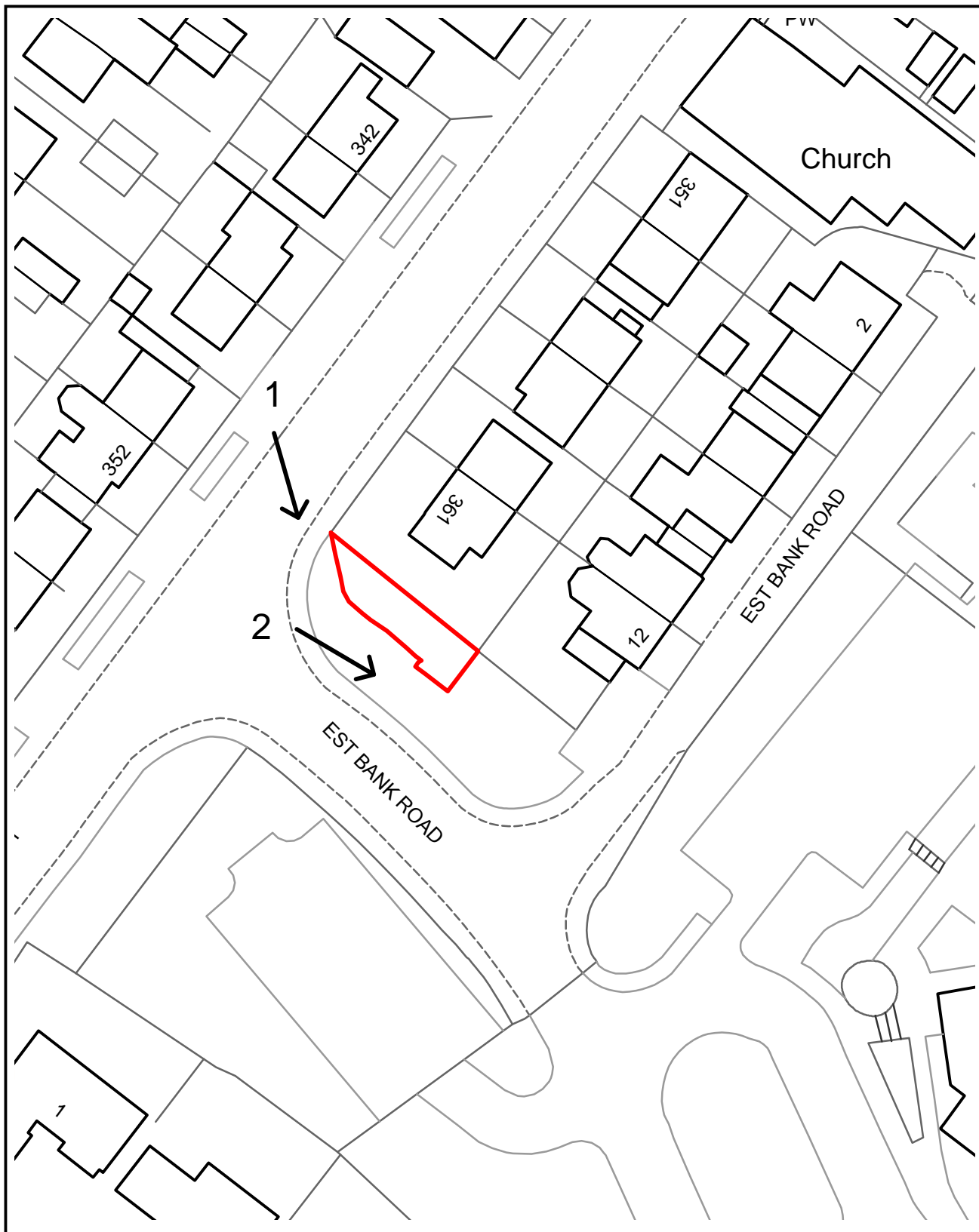
Recommendation: Approve with Conditions

Conditions/ Reasons

1. This decision relates to drawings numbered 2016/142-01A and the development shall not be carried out except in accordance with the drawings hereby approved.
Reason. For the avoidance of doubt and to ensure a satisfactory standard of design pursuant to the policies of the Bury Unitary Development Plan listed.
2. Within 14 days of the date of this decision, the boundary fence hereby approved shall be lowered to a maximum height of 1m along the south east boundary, between the points marked 'x' on the approved plan 2016/142-01/B. The fence shall be maintained at this height thereafter to the satisfaction of the Local Planning Authority.
Reason. In the interests of visual amenity and highway safety pursuant to UDP Policy EN1/2 Townscape and Built Design and HT2 Highway Network.

For further information on the application please contact **Tom Beirne** on **0161 253 5361**

Viewpoints



PLANNING APPLICATION LOCATION PLAN

APP. NO 60879

**ADDRESS: Land Adjacent to 361 Bolton Road West
Ramsbottom**



Planning, Environmental and Regulatory Services

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60879

Photo 1



Photo 2





IMAGE INDICATING FENCE THAT HAS BEEN ERECTED TO DEFINE THE INCREASE IN RESIDENTIAL CURTILAGE



IMAGE OF GRASS ONE TO ACCESS ROAD WITH NEW FENCING INDICATED

Client			
Vortre Williams			
Project			
Proposed extension of residential curtilage to land next to 361 Bolton Road West, Ramsbottom, Bury BL0 9QZ			
Job No.	Area	File Ref	
2016/142			
Title			
Site Plan			
2016/142-01			rev. B
Date	Drn.	Scale	Checked
26.10.16	B. Edmondson	NTS	

eds
edmondson
design services

20 Fernhill Drive
Stacksteads, Bacup
Lancashire
OL13 8JS
mob: 07528809176 tel 01706 870944
e:info@edmondsondesignservices.co.uk

Ward: Prestwich - Holyrood

Item 12

Applicant: Mr Z N Hamfaraj

Location: 478 Bury Old Road, Prestwich, Manchester, M25 1NL

Proposal: Alterations to shop front, conversion from 1 no. unit to 2 no. units and change of use from retail (Class A1) to retail and hot food takeaway (Class A1 & A5) with flue at rear

Application Ref: 60929/Full

Target Date: 31/01/2017

Recommendation: Approve with Conditions

Description

The application relates to a two storey terraced commercial property on the north side of Bury Old Road between the junctions with Polefield Approach and Heywood Road. The site lies within a designated Local Shopping Centre.

The previous use was as a shop (off-licence) but the premises have been vacant for at least 6 months. There is a clothes boutique immediately to the west at 482/484 and a Chinese take-away to the east at 476. There are residential properties to the front, across Bury Old Road and to the rear on Polefield Grange. A service road with on street parking runs along the front of the shops.

It is proposed to split the premises on the ground floor (100sqm) into a hot food takeaway on one side (approx 75sqm) and a barber shop on the other side (approx 25sqm). To facilitate the split, a new aluminium framed shopfront would be installed to the front. A flue for the hot food takeaway would be installed at the rear. The upper floor would be used as ancillary storage. The existing roller shutter would be retained and flue at the rear would be powder coated grey. The existing yard/bin store would remain to service the new units and the upper floor would remain as ancillary storage.

The proposed hours of opening for the takeaway have been revised to:

Monday - Thursday: 12 noon - 11pm

Friday - Sunday: 12 noon - 12 midnight

Relevant Planning History

None relevant.

Publicity

The following neighbours were notified by letter dated 09/12/2016. Nos.472 - 48(even), 471 - 479(odd) Bury Old Road, 32 - 42(even) Polefield Grange, 2 Polefield Approach.

One objection has been received from the occupier/owner of 454 Bury Old Road and set out below:

- I thought the policy of the government was not to allow more than 4 takeaways in the specified mile. There are more than 20 in less than a 1/2 mile and 5 in the immediate area. Clearly shows that council trying to recover its budget through issuing more licenses without considering the existing welfare of the other businesses.

The objector has been notified of the Planning Control Committee meeting.

Consultations

Traffic Section - No objection.

Drainage Section - No objection.
Environmental Health - No objection.
Waste Management - No objection.

Unitary Development Plan and Policies

S2/5 New Local Shopping Provision
S2/6 Food and Drink
EN1/2 Townscape and Built Design
EN1/8 Shop Fronts
NPPF National Planning Policy Framework

Issues and Analysis

The following report includes analysis of the merits of the application against the relevant policies of both the National Planning Policy Framework (NPPF) and the adopted Bury Unitary Development Plan (UDP) together with other relevant material planning considerations. The policies of the UDP that have been used to assess this application are considered to be in accordance with the NPPF and as such are material planning considerations. For simplicity, just the UDP Policy will be referred to in the report, unless there is a particular matter to highlight arising from the NPPF where it would otherwise be specifically mentioned.

Policy - UDP Policy S1/4 Local Shopping Centres states that the Council will seek to maintain and enhance local shopping centres and will encourage the provision of a range of shopping facilities required to serve purely local needs.

Policy S2/4 Control of Non-Retail Uses in All Other Areas states that outside the main shopping area of town and district centres, changes of use, or redevelopment, from a retail shop (Class A1) to a non-retail use will be permitted subject;

- the proposal is appropriate in scale and character to the requirements of the area and necessary to serve local needs which would not otherwise be met;
- the new non-retail use would not result in an over concentration or grouping of uses which could result in a long term loss of trade from a centre compared with a continued Class A1 use;
- the length of time the unit has remained vacant and the viability of a continued Class A1 use;
- whether or not the locality is adequately served by alternative local shopping facilities within reasonable walking distance;
- that a display window is retained or provided, where appropriate;
- that access is provided for the mobility impaired, where appropriate;
- that adequate provision is made for car parking and servicing.

Policy S2/6 Food and Drink states the Council in considering all proposals which involve restaurants, hot food takeaways, cafes, snack bars, wine bars and public houses, together with any other uses contained within Class A3, will have regard to the following factors:

- the amenity of nearby residents by reason of noise, smell, litter and opening hours;
- whether or not the proposal would result in an over concentration of hot food and drink uses, which could adversely change the nature or character of a centre as a whole;
- parking and servicing provision associated with the proposed development and its effects in terms of road safety, traffic generation and movement;
- provision for the storage and disposal of refuse and customer litter;
- the environmental impact of any ventilation flues and/or ducting.

Use - The property is within a Local Shopping Centre and although there are four takeaways on the row, there is a relatively healthy proportion of A1 shops within the centre. Given that the property has been vacant for at least 6 months and that the change of use includes a new barbers shop (class A1) in addition to the hot food takeaway, the proposal would be considered, on balance, acceptable in principle.

Visual amenity and Streetscape - Currently the vacant, shuttered up property does not add anything positive to the character of the shopping centre or streetscape in general.

The proposal would bring the vacant property back into use and, with the new shopfront, would improve the appearance of the property and the immediate streetscape. The proposal would be acceptable in terms of visual amenity and comply with UDP Policies EN1/2 Townscape and Built Design and EN1/8 Shopfronts.

Residential amenity - The opening hours of the takeaway to 11pm Monday to Thursday and Midnight Fridays to Sunday within the Local Shopping Centre on a relatively busy commercial stretch of Bury Old Road would not be unreasonable. The premises are relatively modest in size and would not create serious noise and disturbance in the locality. The proposed flue would run up the rear elevation and extract fumes/odours above the eaves. There is no residential accommodation above. The proposal is acceptable in terms of residential amenity and complies with UDP Policy S2/6 Food and Drink.

Traffic - There are no highway safety issues arising and as such the traffic team have no objections. The scheme complies with UDP Policies S2/6 Food and Drink and HT2/4 Car Parking and New Development.

Statement in accordance with Article 35(2) Town and Country Planning (Development Management Procedure) (England) (Amendment) Order 2015

The Local Planning Authority worked positively and proactively with the applicant to identify various solutions during the application process to ensure that the proposal comprised sustainable development and would improve the economic, social and environmental conditions of the area and would accord with the development plan. These were incorporated into the scheme and/or have been secured by planning condition. The Local Planning Authority has therefore implemented the requirement in Paragraphs 186-187 of the National Planning Policy Framework.

Recommendation: Approve with Conditions

Conditions/ Reasons

1. The development must be begun not later than three years beginning with the date of this permission.
Reason. Required to be imposed by Section 91 Town & Country Planning Act 1990.
2. This decision relates to drawings numbered 1/2 and 2/2 and the development shall not be carried out except in accordance with the drawings hereby approved.
Reason. For the avoidance of doubt and to ensure a satisfactory standard of design pursuant to the policies of the Bury Unitary Development Plan listed.
3. The hot food takeaway use hereby permitted shall not be open to customers outside the following times:
1200hrs to 2300hrs Monday to Thursday.
1200hrs to 12 midnight Friday to Sunday.
Reason. To safeguard the amenities of the occupiers of nearby residential accommodation pursuant to UDP Policy S2/6 Food and Drink.
4. No development shall commence unless and until a scheme for treating, diluting and dispersing fumes and odours has been submitted to and approved in writing by the Local Planning Authority, which shall include:
a written statement from a suitably qualified person that is a member of the Heating and Ventilation Contractors Association (HVCA) or similar body, which demonstrates compliance with the measures proposed in the Guidance on the

Control of Odour and Noise from Commercial Kitchen Exhaust Systems :DEFRA 2005 (or if applicable such superseding guidance as shall prevail at the time of commencement of the development); and
the relevant manufacturer and installer instructions for any associated equipment with details of maintenance requirements .

The scheme as approved shall be implemented, available for use and maintained in accordance with the approved scheme whilst it shall serve the development.

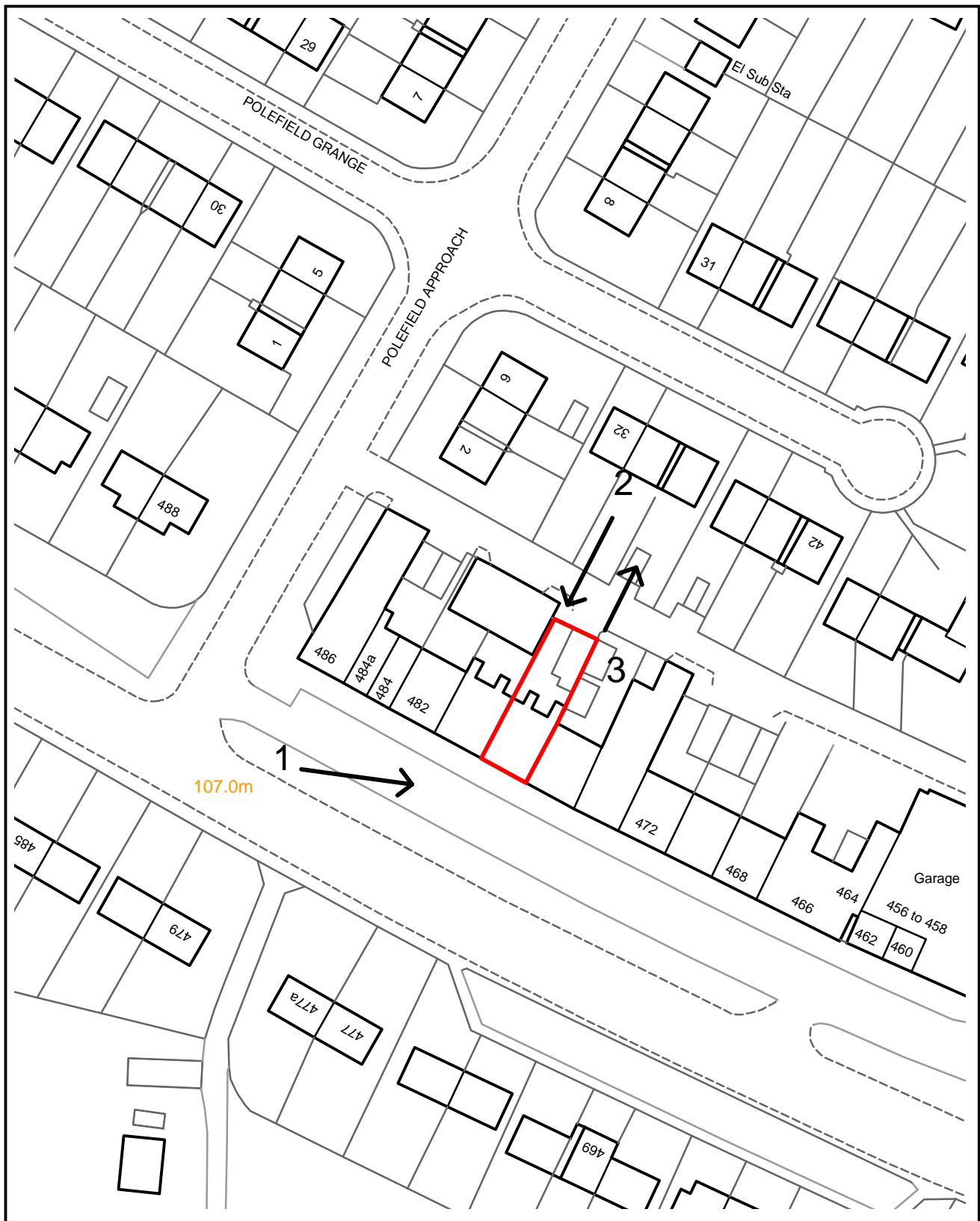
Reason. To protect the residential amenities of nearby residential property from impact upon from fumes and odour pursuant to UDP Policy S2/6 - Food and Drink.

5. The proposed flue at the rear shall be finished in a powder coated dark grey and maintained in that colour thereafter.

Reason. In the interests of visual amenity pursuant to UDP Policy EN1/2 Townscape and Built Design.

For further information on the application please contact **Tom Beirne** on **0161 253 5361**

Viewpoints



PLANNING APPLICATION LOCATION PLAN

APP. NO 60929

**ADDRESS: 478 Bury Old Road
Prestwich**

Planning, Environmental and Regulatory Services

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Bury
COUNCIL

60929

Photo 1

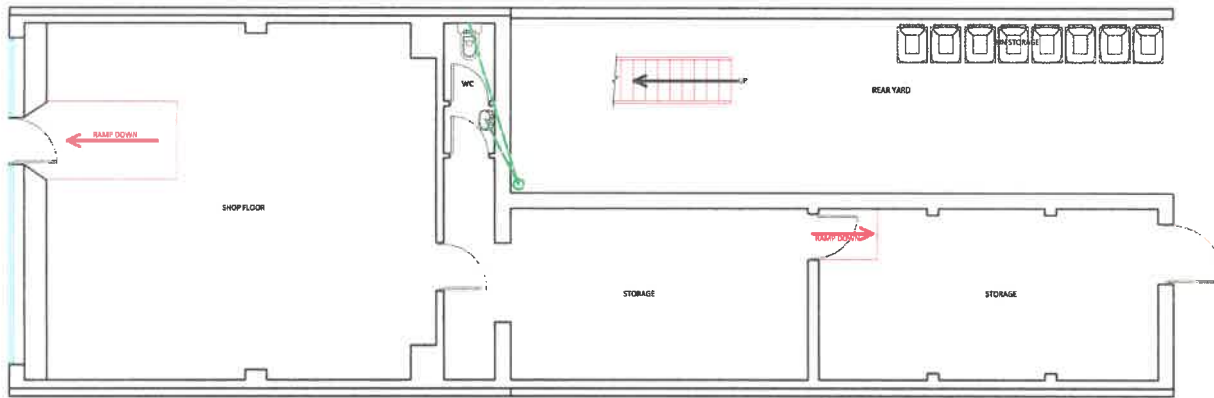


Photo 2

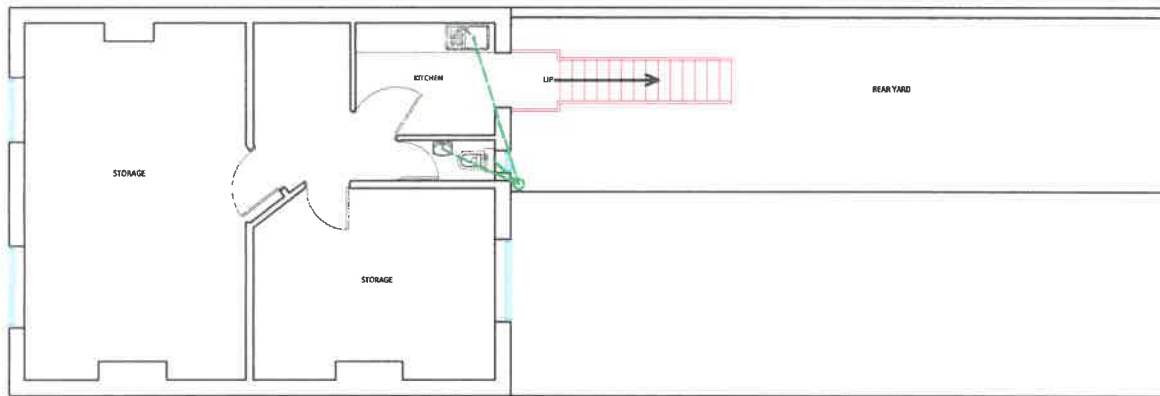


Photo 3

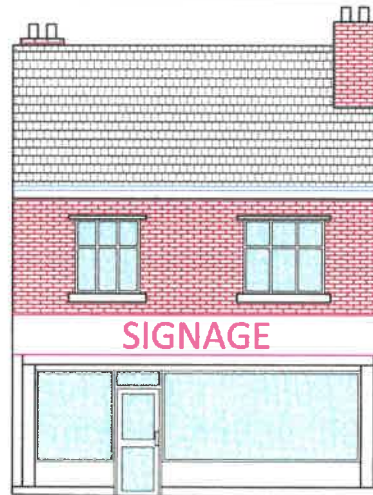




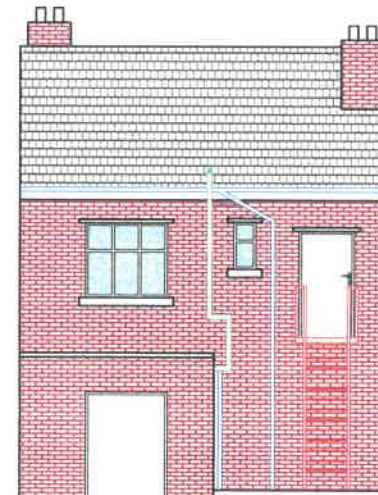
EXISTING GROUND FLOOR PLAN



EXISTING FIRST FLOOR PLAN




EXISTING FRONT ELEVATION



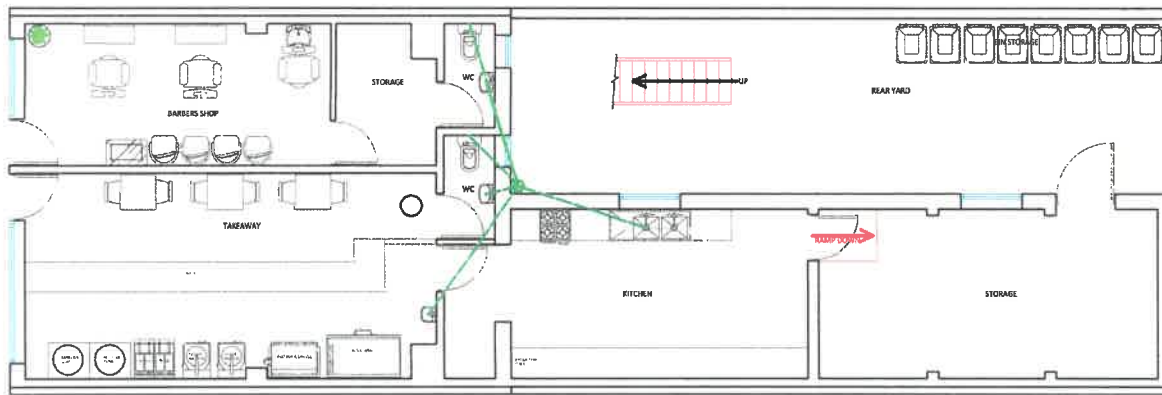
EXISTING REAR ELEVATION

REV	DATE	AMENDMENTS
CLIENT	MR ZANA NASRADEEN HAMFAAI	
DRAWN BY	SK	
DATE	21 NOVEMBER 2016	
SCALE	1:00 @ A3	
PROJECT REF	00130	
DRAWING No	172	
TITLE:		
PROPOSED SPLITTING 1 RETAIL UNIT TO 2 AND CHANGE OF USE ON 1 UNIT FROM A1 TO AS AT: 478 BURY OLD ROAD MANCHESTER M25 1NL		
<small>(This drawing shall be printed upon dimensions listed at the start of this drawing unless so indicated) (THIS DRAWING IS COPYRIGHT © SKILL NET REPRODUCES WITHOUT PERMISSION)</small>		

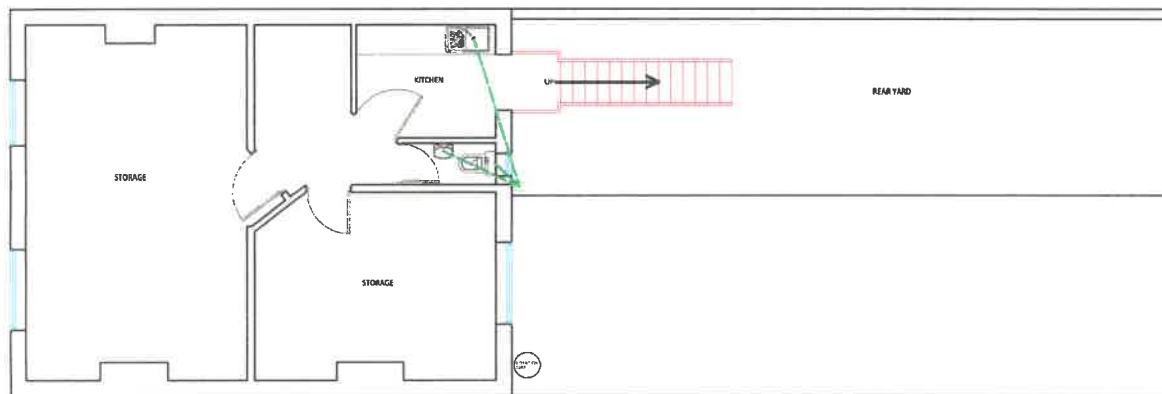


A | ARCHITECTURAL SERVICES

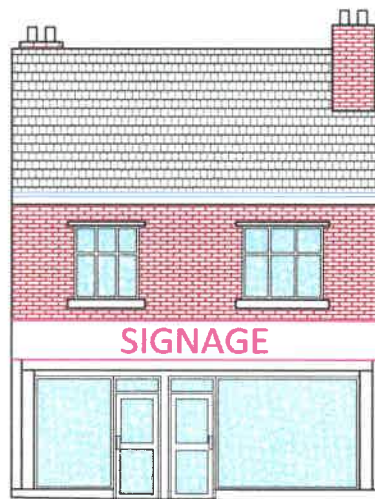
**90r BURY OLD ROAD, MANCHESTER, M6 5BW
TEL: 0161 7959980 MOB: 07771868293
EMAIL: info@a3architecturalservices.co.uk**



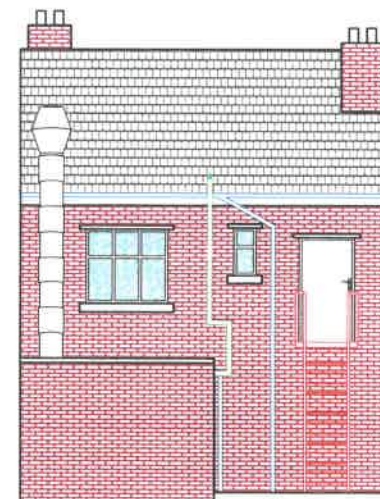
PROPOSED GROUND FLOOR PLAN




PROPOSED FIRST FLOOR PLAN



PROPOSED FRONT ELEVATION



PROPOSED REAR ELEVATION

REV	DATE	AMENDMENTS
CLIENT		MR ZANA NASRADEEN HAMFARAJ
DRAWN BY		SK
DATE		21 NOVEMBER 2016
SCALE		1:00 @ A3
PROJECT REF		00110
DRAWING No		2/2
TITLE:		
PROPOSED SPLITTING 1 RETAIL UNIT TO 2 AND		
CHANGE OF USE ON 1 UNIT FROM A1 TO A5 AT:		
478 BURY OLD ROAD		
MANCHESTER		
M25 1NL		
<small>(This document should be placed upon dimensions which are scaled off one drawing (please see dimensions))</small>		
<small>(THIS DRAWING IS COPYRIGHT & SHALL NOT BE REPRODUCED WITHOUT PERMISSION)</small>		
		
A ARCHITECTURAL SERVICES		
90a BURY OLD ROAD, MANCHESTER, M8 5BW		
TEL: 0161 7959980 MOB: 07771868293		
EMAIL: info@a3architecturalservices.co.uk		

Ward: Bury East

Item 13

Applicant: Mr M Azeem

Location: Land adjacent to 32 Nuttall Street, Bury, BL9 7EW

Proposal: Erection of 1 no. attached dwelling

Application Ref: 60939/Full

Target Date: 03/02/2017

Recommendation: Approve with Conditions

Description

The application relates to a vacant strip of land located on the corner of Nuttall Street and Alfred Street at the end of a row of terraced dwellings. The immediate area is characterised by residential properties, with rows of terraced properties to the north, south and east of the site and a mixture of semi-detached and detached bungalows and two storey dwellings to the west.

Planning permission is sought for the construction of a single attached dwelling at the end of a row of terraces. The proposed dwelling would project a maximum of 4370 mm from the side elevation of the adjacent property with the principal front and rear elevations running flush with the front and rear of the this dwelling. To the rear it is proposed to construct a single storey element, that would project approximately 3 metres, with an area of private residential amenity space provided within an enclosed rear yard. A rear box dormer is also proposed.

Relevant Planning History

60629 - Erection of 1 no. new dwelling - Withdrawn - Invalid 08/12/2016

Publicity

Letters sent to 11 neighbouring properties 12/12/2016 - one objection received from No. 32 with concerns regarding:

- Purchased their dwelling as an end terrace in order to reduce potential noise disturbance.
- Parking in the area is on road and additional cars would restrict parking for existing residents.
- Noise caused during construction.
- Restricted access to the rear yard of No. 32 during construction.
- Possible damage to their property caused during construction.
- Issues with previous building movement and the possibility of the proposal creating more structural problems for No.32
- They have not been approached by the applicant with regards to how they intend to attach the proposed dwelling to their property and what works may be required in order to complete works.

Consultations

Traffic Section - No objection subject to a condition to ensure that highway remedial works around the perimeter of the site required as the result of construction are implemented.

Borough Engineer - Drainage Section - No comment

Environmental Health Contaminated Land - No objections raised subject to conditions

relating to a preliminary risk assessment, site investigation, detailed risk assessment and remediation strategy and the implementation of the remediation strategy and site verification report.

Waste Management (David Pascoe) - No comment

Property & Technical Services - Estates Consultancy - No comment

United Utilities (Water and waste) - No comment

Unitary Development Plan and Policies

H2/1	The Form of New Residential Development
H2/2	The Layout of New Residential Development
EN1/2	Townscape and Built Design
SPD6	Supplementary Planning Document 6: Alterations & Extensions
H5/1	Area Improvement
H2/3	Extensions and Alterations
H1/2	Further Housing Development
NPPF	National Planning Policy Framework

Issues and Analysis

The following report includes analysis of the merits of the application against the relevant policies of both the National Planning Policy Framework (NPPF) and the adopted Bury Unitary Development Plan (UDP) together with other relevant material planning considerations. The policies of the UDP that have been used to assess this application are considered to be in accordance with the NPPF and as such are material planning considerations. For simplicity, just the UDP Policy will be referred to in the report, unless there is a particular matter to highlight arising from the NPPF where it would otherwise be specifically mentioned.

Policy

Policy H2/1- The Form of New Residential Development requires all new residential development to make a positive contribution to the form and quality of the surrounding area.

Policy EN1/2 - Townscape and Built Design seeks to ensure that any proposals would not have an adverse impact on the particular character and townscape of an area.

Policy H2/2 - The Layout of New Residential Development seeks to ensure that new residential development will be acceptable in terms of layout in order to provide a good quality residential environment.

Policy HT2/4 - Car Parking and New Development requires all applications for development to make adequate provision for their car parking and servicing requirements.

Policy H1/2 - Further Housing Development requires the council to have regard for the suitability of proposals for housing development on sites not identified on the Proposals Map.

It should also be noted that the site is located within the Pimhole area of Bury which the UDP identifies as suffering from a concentration of poor housing. Policy H5/1- Area Improvement therefore seeks to improve house condition and housing environment in this area.

Principle

Taking into consideration that the site is located within an established residential area the principle of a new house on the site is considered to be acceptable in terms UDP Policies H1/2 Further Housing Development and H1/2 - Further Housing Development.

Visual amenity

The proposed house would, next to the existing terrace, appear as an end terrace of similar

proportions to the adjacent dwellings. It would be modest in scale and finished in facing brick and slate to match a number of the surrounding terraces. The proposed window and door openings to front and rear would be suitably aligned, and replicate the style and form of the attached building. The proposed dwelling is considered to be in keeping with the site and surroundings.

In this instance therefore the proposal is acceptable in terms of visual amenity and complies with UDP Policies H2/1- The Form of New Residential Development, EN1/2 -Policy EN1/2 - Townscape and Built Design and H2/2 - Policy H2/2 - The Layout of New Residential Development.

Residential amenity

There are no adopted aspect standards for new build residential properties however, Supplementary Planning Document 6 provides guidance on aspect standards between residential properties and as such, would be a reasonable guide in this case.

SPD 6 requires a distance of 20 metres between directly facing habitable room windows. There is a distance of approximately 18 metres between the proposed dormer and proposed rear first floor bedroom windows, and the rear elevation of No. 33 Oxford Street. Whilst this distance is below the aspect standards required, SPD 6 does state that "in a terraced street, where current aspect standards are not available, the existing distances between habitable room windows will be maintained as a minimum." Taking into consideration that the proposal maintains existing distances between dwellings within the terraces, and the fact that the terraces in these rows could construct a similar dormer window under permitted development rights, in this instance the proposal is considered to be acceptable.

The rear ground floor window relates to a non-habitable room (kitchen) and as such aspect standards do not apply. In addition to this the window to this room would be screened by the proposed 2 metre boundary wall which extends above eyeline when viewed from within the proposed kitchen. In this instance therefore the proposed rear kitchen windows are considered to be acceptable.

As with the rear windows, the distance between the front habitable room windows and the directly facing property at No. 35 Nuttall Street falls below the 20 metres required within SPD 6. Again, taking into consideration that the proposal maintains existing distances between dwellings within the terraces in this instance the proposal is considered to be acceptable.

The proposed first floor, and second floor side windows relate to non-habitable rooms and as such a condition should be attached to ensure these windows are obscure glazed to prevent any possible overlooking of No. 16.

The proposed side ground floor window relates to a dining room and faces towards the side elevation of No. 16 which has a single bathroom window that faces towards the site. A bathroom window is considered to be non-habitable and as such is not afforded the same protection through Policy that habitable room windows are. In this instance given the use of the window of No. 16, the concrete boundary fence and the road that divides the two properties it is considered that the proposed dining room window would not have a detrimental impact on the residential amenity of No.16.

In this instance it is considered that the proposal complies with UDP Policy H2/1- The Form of New Residential Development.

Highways

No objection has been raised in relation to the proposal from the Traffic Section subject to a condition being attached in relation to remedial works required to the adopted highway. The proposal would be located within an established residential area with on street parking available for residents, which the proposal site intends to utilise. The site is located within a

sustainable location with links to public transport. In this instance it is considered given the nature of the existing parking provision in the area the proposal is acceptable.

Neighbour Objections

Matters controlled under non-planning legislation, private issues between neighbours, damage caused through construction and problems arising from the construction period are non-material planning considerations that are not relevant to the decision.

The applicant has signed Certificate B in relation to the planning application which certifies that the required notice has been given to the residents of No. 32 Nuttall Street in relation to the planning application.

Statement in accordance with Article 35(2) Town and Country Planning (Development Management Procedure) (England) (Amendment) Order 2015

The Local Planning Authority worked positively and proactively with the applicant to identify various solutions during the application process to ensure that the proposal comprised sustainable development and would improve the economic, social and environmental conditions of the area and would accord with the development plan. These were incorporated into the scheme and/or have been secured by planning condition. The Local Planning Authority has therefore implemented the requirement in Paragraphs 186-187 of the National Planning Policy Framework.

Recommendation: Approve with Conditions

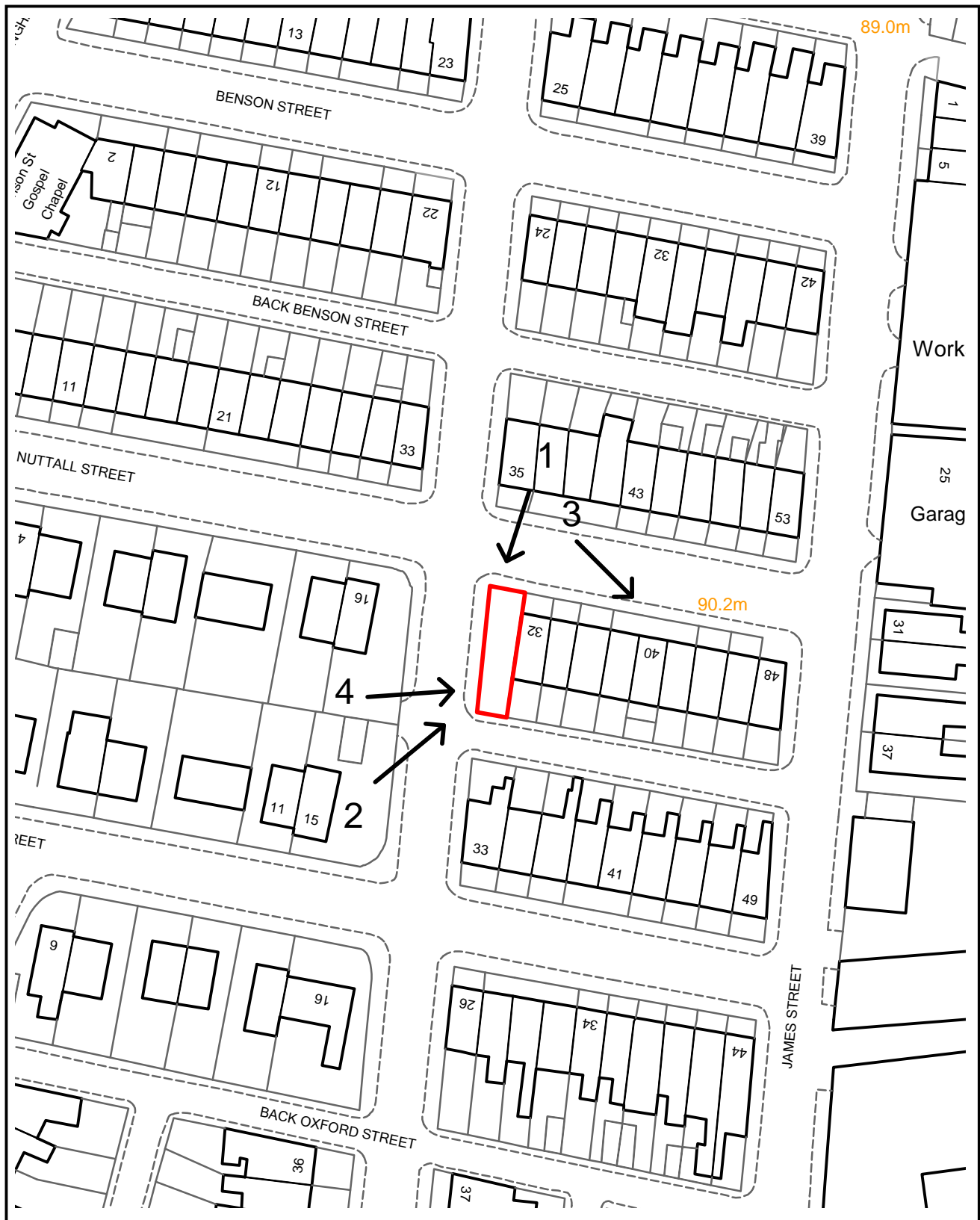
Conditions/ Reasons

1. The development must be begun not later than three years beginning with the date of this permission.
Reason. Required to be imposed by Section 91 Town & Country Planning Act 1990.
2. This decision relates to drawings numbered RAD/1502/16/1/RevB and the development shall not be carried out except in accordance with the drawings hereby approved.
Reason. For the avoidance of doubt and to ensure a satisfactory standard of design pursuant to the policies of the Bury Unitary Development Plan listed.
3. Details/Samples of the (materials/bricks) to be used in the external elevations, together with details of their manufacturer, type/colour and size, shall be submitted to and approved in writing by the Local Planning Authority before the development is commenced. Only the approved materials/bricks shall be used for the construction of the development.
Reason. No material samples have been submitted and are required in the interests of visual amenity and to ensure a satisfactory development pursuant to UDP Policy EN1/1 Visual Amenity.
4. Before the first occupation of the dwelling hereby permitted the first floor and second floor side windows shall be fitted with obscured glazing and shall be permanently retained in that condition thereafter.
Reason. To protect the privacy of adjoining occupiers and to accord with Policy H2/3 - Extensions and Alterations of the Bury Unitary Development Plan and Supplementary Planning Document 6 - Alterations and Extensions to Residential Properties.

5. Before the first occupation of the dwelling hereby permitted the ground floor side window shall be fitted with glazing that does not open outwards onto the adjacent adopted highway and shall be permanently retained in that condition thereafter.
Reason. To ensure that the adopted highway is maintained free of obstacles pursuant of Policy HT6/1 - Pedestrian and Cyclist Movement.
6. No development shall commence unless and until:-
- A contaminated land Preliminary Risk Assessment report to assess the actual/potential contamination and/or ground gas/landfill gas risks at the site shall be submitted to, and approved in writing by, the Local Planning Authority;
 - Where actual/potential contamination and/or ground gas/landfill gas risks have been identified, detailed site investigation and suitable risk assessment shall be submitted to, and approved in writing by the Local Planning Authority;
 - Where remediation/protection measures is/are required, a detailed Remediation Strategy shall be submitted to, and approved in writing by, the Local Planning Authority.
- Reason. The scheme does not provide full details of the actual contamination and subsequent remediation, which is required to secure the satisfactory development of the site in terms of human health, controlled waters, ground gas and the wider environment and pursuant to National Planning Policy Framework Section 11 - Conserving and enhancing the natural environment.
7. Following the provisions of Condition 6 of this planning permission, where remediation is required, the approved Remediation Strategy must be carried out to the satisfaction of the Local Planning Authority within agreed timescales; and A Site Verification Report detailing the actions taken and conclusions at each stage of the remediation works, including substantiating evidence, shall be submitted to and approved in writing by the Local Planning Authority prior to the development being brought into use.
Reason. To secure the satisfactory development of the site in terms of human health, controlled waters and the wider environment and pursuant to National Planning Policy Framework Section 11 - Conserving and enhancing the natural environment.
8. The development hereby approved shall not be first occupied unless and until all highway remedial works around the perimeter of the site required as a result of the construction of the proposed dwelling and boundary walls have been implemented.
Reason. To ensure good highway design and maintain the integrity of the adopted highway, in the interests of highway safety.

For further information on the application please contact **Helen Goldsbrough** on **0161 253 5277**

Viewpoints



PLANNING APPLICATION LOCATION PLAN

APP. NO 60939

ADDRESS: Land adj 32 Nuttall Street
Bury

Planning, Environmental and Regulatory Services

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Bury
COUNCIL

60939

Photo 1



Photo 2

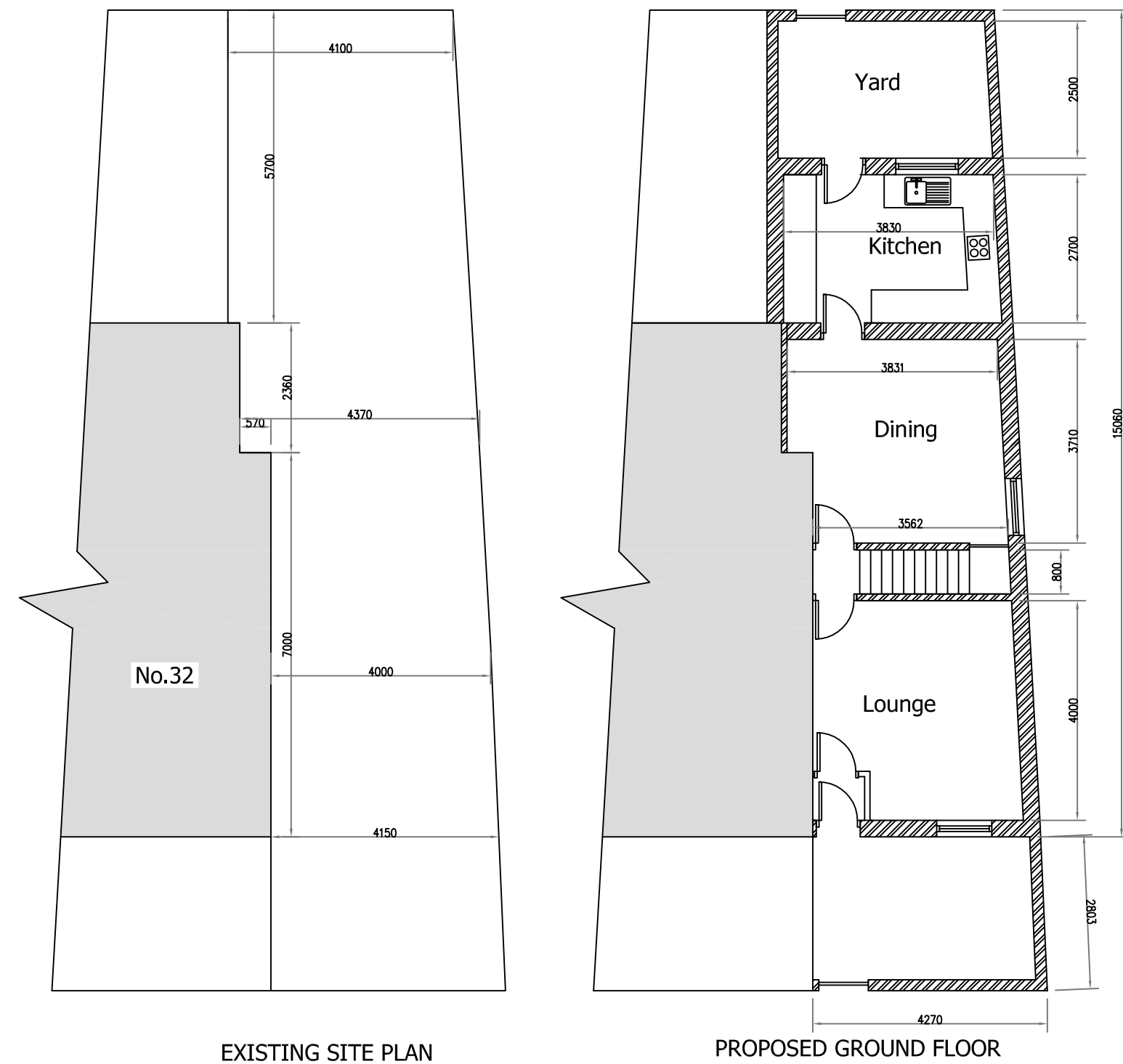


Photo 3



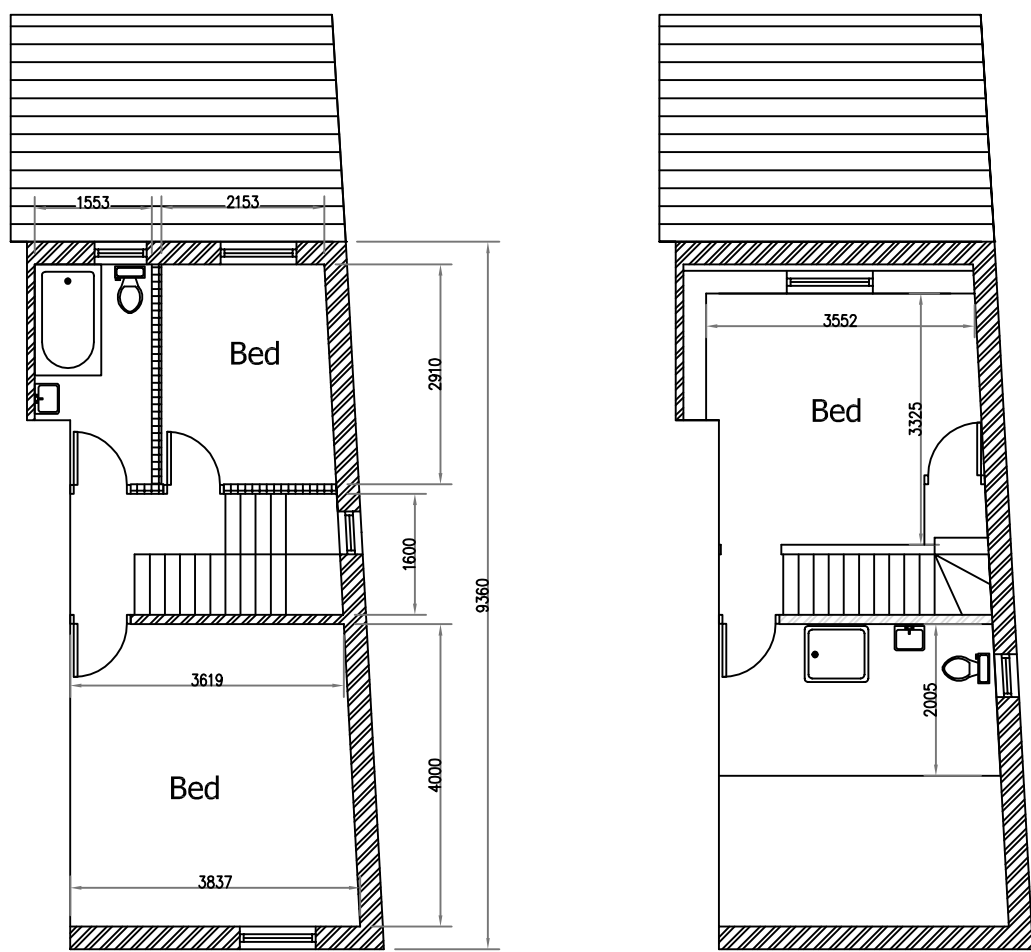
Photo 4





EXISTING SITE PLAN

PROPOSED GROUND FLOOR



PROPOSED FIRST FLOOR

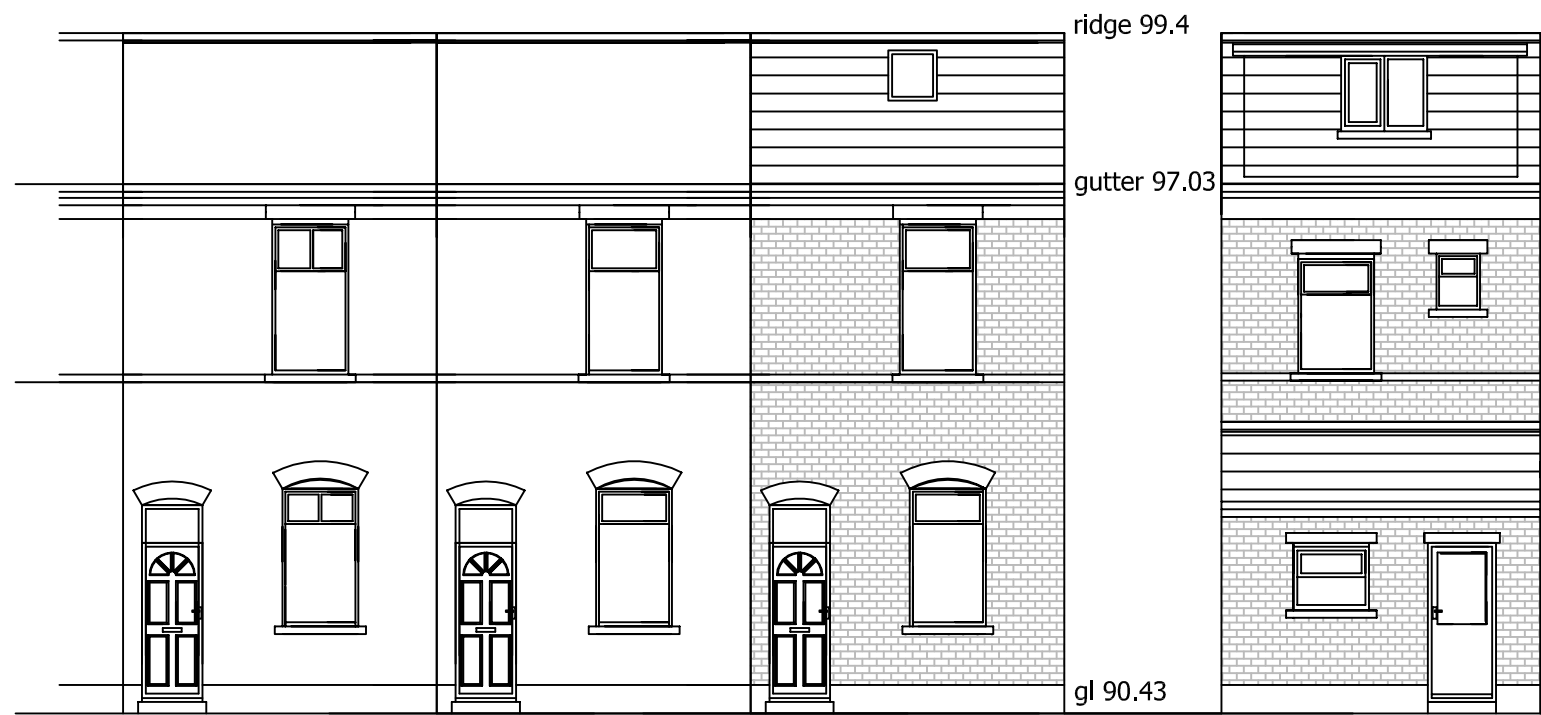
PROPOSED SECOND FLOOR



LOCATION PLAN (1:1250)
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Ordnance Survey 0100031673

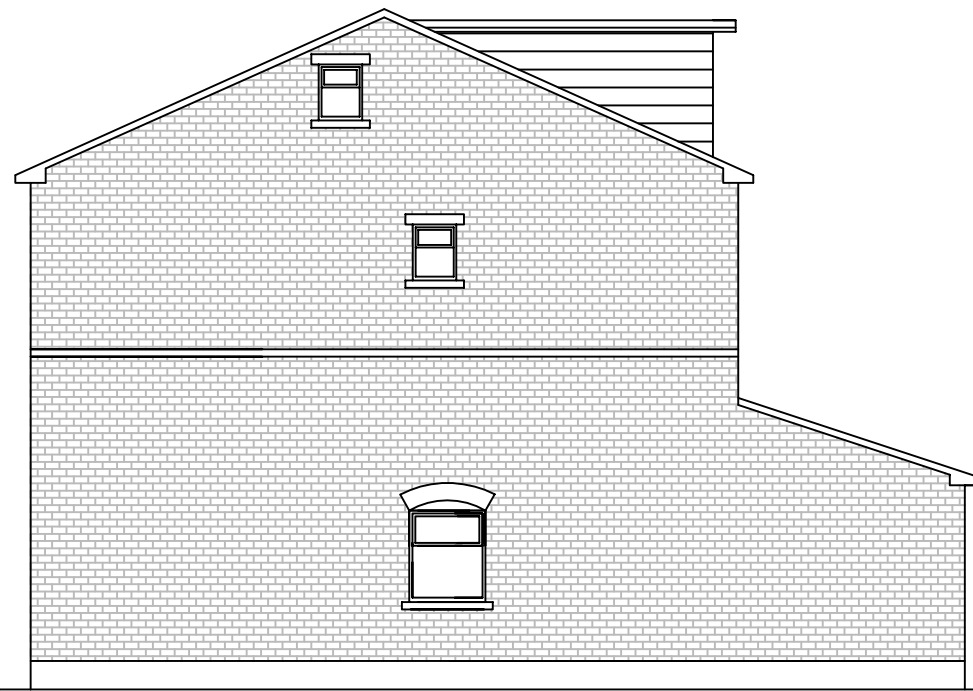


BLOCK PLAN (1:500)

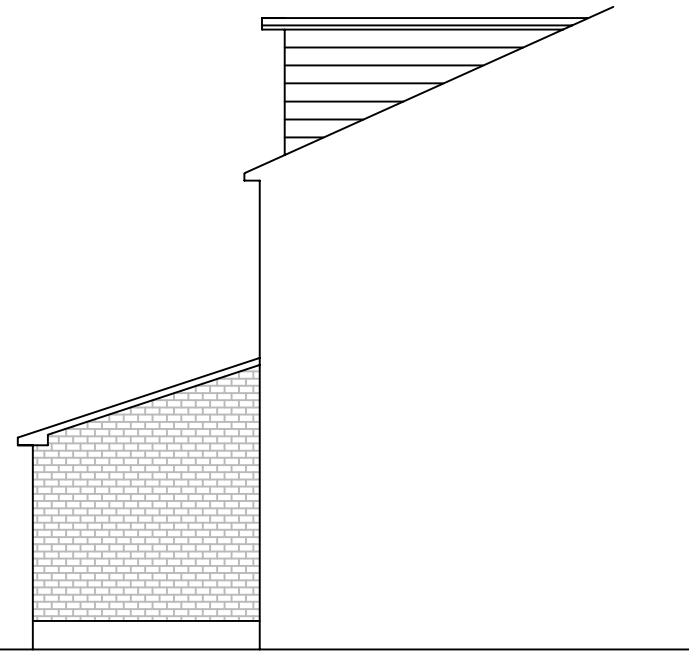


PROPOSED FRONT ELEVATION

PROPOSED REAR ELEVATION



PROPOSED SIDE ELEVATION



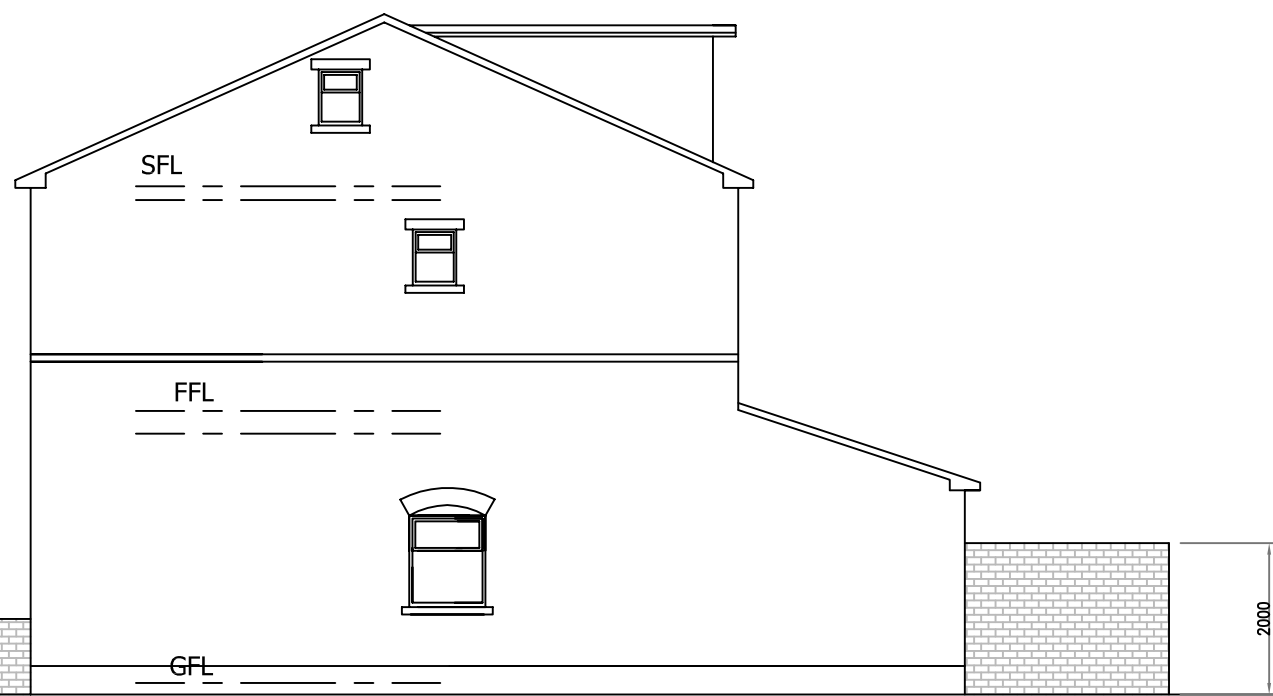
PROPOSED SIDE ELEVATION



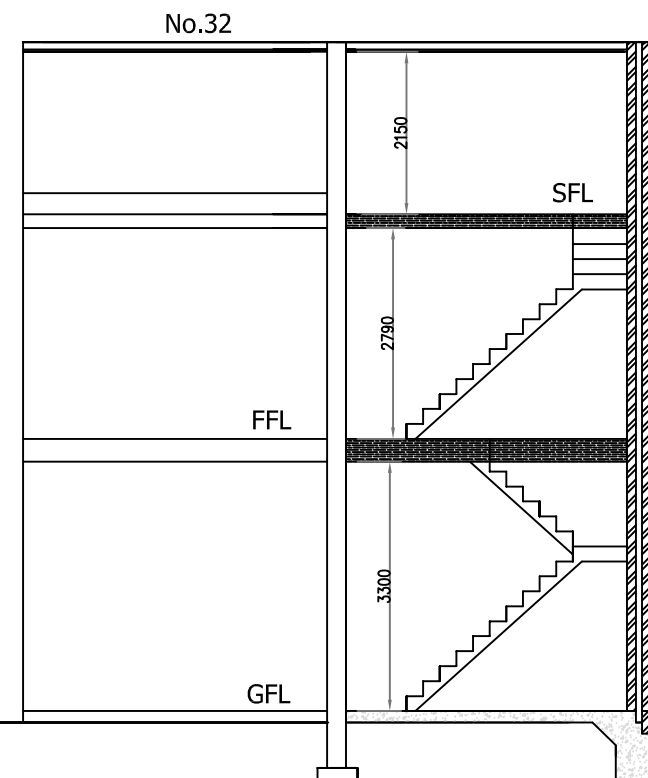
PROPOSED BOUNDARY WALL
FRONT

PROPOSED BOUNDARY WALL
REAR

Boundary Walls :
Brick
Front Gate : Metal
Rear Gate :
Timber



PROPOSED SIDE BOUNDARY WALL



SECTION



SITE PLAN (1:200)

Date	Rev.		Initials
2/11/16	A.	Levels, section, details added	RA
9/1/17	B.	Edge beam foundation added	RA

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Figured dimensions take precedence over scaled measurements from the drawing. All dimensions to be checked by Contractor and discrepancies clarified with the Client prior to commencement.
The whole of the works to comply with current Building Regulations. No work to commence without Building Regulations approval.
The client must ensure the project complies with the Construction Design and Management Regulations 2015

Notes

RAD

The White House,
42-44 Chorley New Road,
Bolton
BL1 4AP

Tel : 01204 322196
Fax : 01204 214110
Mob: 07790 361104
Email: info@radesignprojects.co.uk

Project

LAND ADJ 32 NUTTALL STREET
BURY
BL9 7EL

Title

EXISTING & PROPOSED PLANS

Client

Drawn RA

Date SEPTEMBER 2016

Scale 1:100

Drawing Number RAD/1502/16/1/RevB

Ward: Bury East

Item 14

Applicant: Fairfield Community Primary School

Location: Fairfield Primary School, Rochdale Old Road, Bury, BL9 7SD

Proposal: Classroom extension and covered corridor; Replacement parking spaces at front

Application Ref: 60957/Full

Target Date: 10/02/2017

Recommendation: Approve with Conditions

Description

The application relates to a Fairfield County Primary School on Rochdale Old Road. The school is set at a lower level than the main road on a site that slopes to the south, with the buildings to the east of the site set at a lower level. The school is largely constructed at single storey, with a flat roof however there is a larger two storey hall located at the front of the school. There is a large playground and playing fields located to the south, and a nursery building located to the west. The site has residential properties to all boundaries.

Planning permission is sought for the removal of 4 parking spaces and for the construction of a single storey classroom extension to the east side elevation of the school. The extension will be constructed within an gap between a classroom and the school canteen. The proposed extension would have a flat roof, and would be constructed from materials to match the existing school. Permission is also sought for the replacement of the 4 parking spaces, and the provision of 3 additional parking spaces to the front of the school, to provide a net gain of 3.

Relevant Planning History

21590 - Detached nursery unit - Approve 15/09/1988

34544 - Single storey classroom and store extension - Approve with Conditions 10/09/1998

40382 - Classroom extension - Approve with Conditions 21/03/2003

49426 - Demolition of existing nursery and erection of replacement 25 place nursery, seminar room and ancillary facilities - Approve with Conditions 16/04/2008

49889 - Installation of temporary metal storage container - Approve with Conditions 19/06/2008

53986 - In-fill extension to the main entrance and entrance canopy. - Approve with Conditions 07/06/2011

59495 - Single storey conservatory extension at rear - Approve with Conditions 11/01/2016

Publicity

Letters sent to 71 neighbouring properties. One objection raised from No. 269 Rochdale Old Road with concerns regarding:

- Lack of parking within the area.
- 7 parking spaces is not enough ease congestion caused by the school.
- A residents parking scheme should be introduced.

Consultations

Traffic Section - No objection subject to a condition that requires the replacement car

parking to be surfaced, demarcated and available for use prior to the development being brought into use.

Borough Engineer - Drainage Section - No comments

Environmental Health Contaminated Land - No objection subject to conditions relating to a contaminated land preliminary risk assessment, site investigation, detailed risk assessment and remediation strategy, implementation of the remediation strategy and site verification report, and ground gas protection measures.

Unitary Development Plan and Policies

EN1/2	Townscape and Built Design
CF2	Education Land and Buildings
HT5/1	Access For Those with Special Needs
SPD6	Supplementary Planning Document 6: Alterations & Extensions

Issues and Analysis

The following report includes analysis of the merits of the application against the relevant policies of both the National Planning Policy Framework (NPPF) and the adopted Bury Unitary Development Plan (UDP) together with other relevant material planning considerations. The policies of the UDP that have been used to assess this application are considered to be in accordance with the NPPF and as such are material planning considerations. For simplicity, just the UDP Policy will be referred to in the report, unless there is a particular matter to highlight arising from the NPPF where it would otherwise be specifically mentioned.

Policy

Policy CF2 - Education Land and Buildings states that the council will, where appropriate, consider favourably proposals for the provision, improvement and dual use of educational facilities.

Policy EN1/2 - Townscape and Built Design seeks to protect the character and townscape of the boroughs towns.

Policy HT5/1 - Access for those with Special Needs seeks to ensure better design of buildings to provide access for those with special needs.

Principle

The proposed extension would provide additional classroom space for the school, where as the parking to the front of the school would replace the existing informal parking lost and provide an additional 3 spaces for the school. It is therefore considered that the proposed extension and replacement car parking would be an improvement to the provision of educational facilities and complies with Policy CF2 - Education Land and Buildings.

Layout

The proposed extension would be located to the side of the school and would not be visible from the public highway. The proposed extension is acceptable in terms of scale and massing and would be no higher than the adjacent school buildings. The flat roof would replicate the style of the existing school buildings, and the windows are of a similar style and appropriate alignment. The use of matching materials is considered to be acceptable and would create a harmonious development.

The creation of parking to the front of the school would result in the loss of an area of soft landscaping. This area is set below the highway (Rochdale Old Road) and as such is well screened from the streetscene. In this instance it is considered that the proposed parking area would not pose any visual amenity issues.

It is considered that the proposal would not have a detrimental impact on the character of the immediate street scene or the existing school building and as such complies with Policy

EN1/2 - Townscape and Built Design.

The layout of the new parking area would not interfere with the accesses into or circulation around the building. The proposed extension would not introduce any changes in floor levels when accessed from within the school and provision has been made for an external ramp to be provided for the new external door. In this instance it is considered that the proposal complies with Policy HT5/1 - Access for those with Special Needs.

Residential Amenity

There are no adopted aspect standards between residential and non-residential uses however, Supplementary Planning Document 6 provides guidance on aspect standards between residential properties and as such, would be a reasonable guide in this case.

SPD 6 requires a minimum distance of 20 metres between directly facing habitable room windows. The closest residential property is No. 25 Fairfield Drive. This property is located approximately 28 metres from the side elevation and windows of the proposed extension. In addition to this the windows will be screened by existing boundary treatments which extend above eyeline. In this instance therefore it is considered that the residential amenity of the neighbouring properties will be maintained.

Parking

The proposed extension is located in an area that is used by the school as parking for approximately 4 vehicles. There are no demarcated parking bays in this area however the applicant has made provision to ensure that this parking is replaced. The proposal seeks to replace this parking and add an additional 3 spaces to the front of the school, including the provision of 2 disabled bays.

SPD 11 seeks 1.5 parking spaces per classroom. The school currently has 9 classrooms, with a further 1 proposed. This equates to a total of 15 spaces. The proposal would lead to a total of 38 spaces provided at the school which far exceeds those required by SPD 11. In this instance therefore it is considered that adequate off street parking has been maintained.

Neighbour Objection

The proposal would result in a net increase in parking at the school and as stated above it is considered that adequate off street parking has been maintained.

The proposal is for a single storey classroom extension, and replacement parking within the school grounds. The red edge for the application only relates to these changes and therefore the Council has no scope to implement or request a residents parking scheme as part of the planning application.

Statement in accordance with Article 35(2) Town and Country Planning (Development Management Procedure) (England) (Amendment) Order 2015

The proposal complies with the development plan and would improve the economic, social and environmental conditions of the area. It therefore comprises sustainable development and the Local Planning Authority worked proactively and positively to issue the decision without delay. The Local Planning Authority has therefore implemented the requirement in Paragraphs 186-187 of the National Planning Policy Framework.

Recommendation: Approve with Conditions

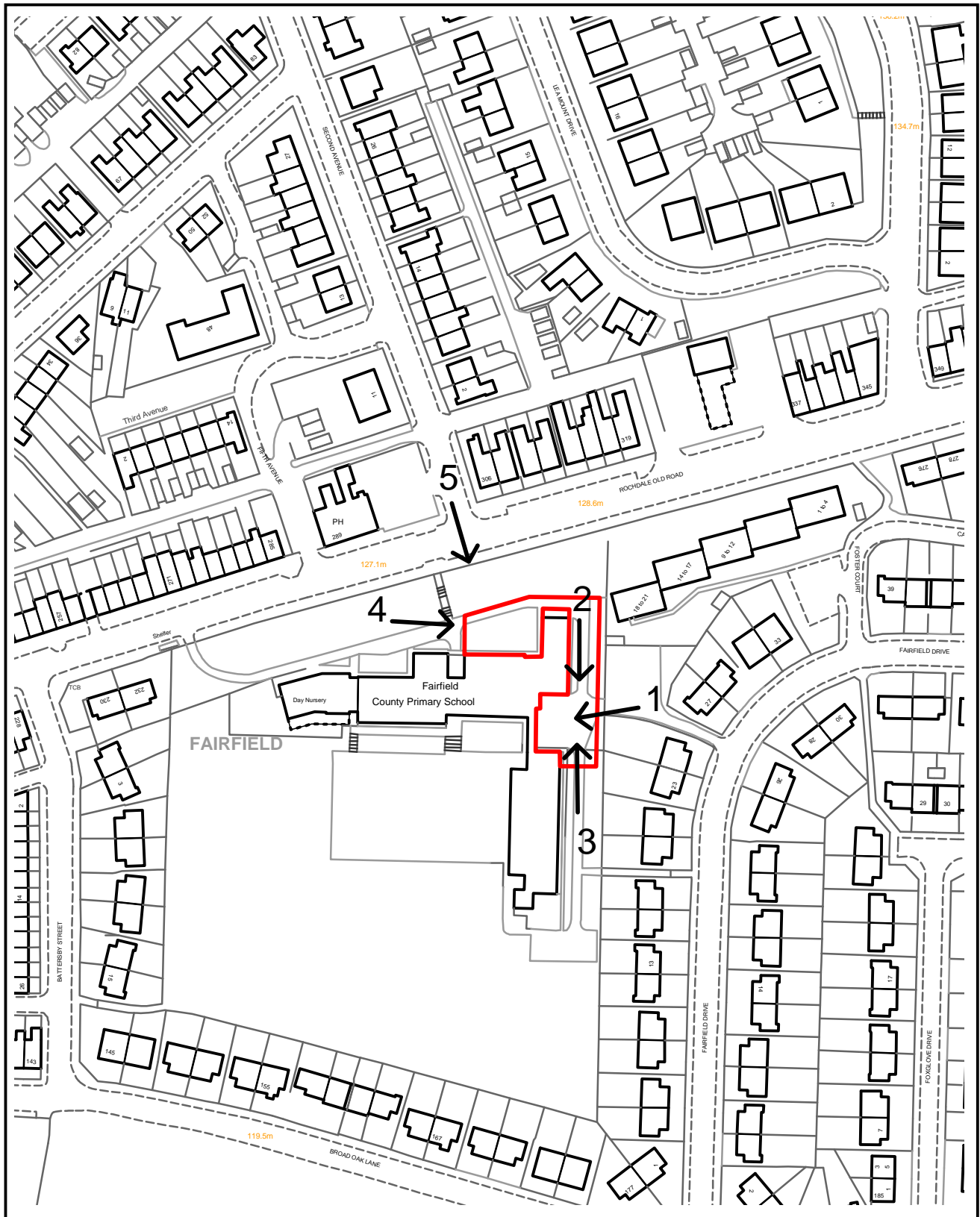
Conditions/ Reasons

1. The development must be begun not later than three years beginning with the date of this permission.
Reason. Required to be imposed by Section 91 Town & Country Planning Act 1990.

2. This decision relates to drawings numbered 16/450/SLP, 16/450.SBP, 16/450.01, 16/450.02, 16/450.03, 16/450.04, 16/450.05 and 16/450.06 and the development shall not be carried out except in accordance with the drawings hereby approved.
Reason. For the avoidance of doubt and to ensure a satisfactory standard of design pursuant to the policies of the Bury Unitary Development Plan listed.
3. The external finishing materials for the proposal hereby approved shall match those of the existing building.
Reason. In the interests of visual amenity and to ensure a satisfactory development pursuant to Policy EN1/2 - Townscape and Built Design of Bury Unitary Development Plan.
4. The replacement car parking indicated on approved plan reference 16/450.05 shall be surfaced, demarcated and made available for use to the written satisfaction of the Local Planning Authority prior to the extension hereby approved being brought into use.
Reason. To ensure adequate off street car parking provision in the interests of road safety pursuant to policy HT2/4 - Car Parking and New Development of the Bury Unitary Development Plan.
5. No development shall commence unless and until:-
 - A contaminated land Preliminary Risk Assessment report to assess the actual/potential contamination and/or ground gas/landfill gas risks at the site shall be submitted to, and approved in writing by, the Local Planning Authority;
 - Where actual/potential contamination and/or ground gas/landfill gas risks have been identified, detailed site investigation and suitable risk assessment shall be submitted to, and approved in writing by the Local Planning Authority;
 - Where remediation/protection measures is/are required, a detailed Remediation Strategy shall be submitted to, and approved in writing by, the Local Planning Authority.Reason. The scheme does not provide full details of the actual contamination and subsequent remediation, which is required to secure the satisfactory development of the site in terms of human health, controlled waters, ground gas and the wider environment and pursuant to National Planning Policy Framework Section 11 - Conserving and enhancing the natural environment.
6. Following the provisions of Condition 5 of this planning permission, where remediation is required, the approved Remediation Strategy must be carried out to the satisfaction of the Local Planning Authority within agreed timescales; and A Site Verification Report detailing the actions taken and conclusions at each stage of the remediation works, including substantiating evidence, shall be submitted to and approved in writing by the Local Planning Authority prior to the development being brought into use.
Reason. To secure the satisfactory development of the site in terms of human health, controlled waters and the wider environment and pursuant to National Planning Policy Framework Section 11 - Conserving and enhancing the natural environment.
7. The measures relating to detailed design features relating to the alleviation of gas ingress shall be submitted to and approved before the extension hereby approved is commenced. The approved measures shall be implemented in full during the construction of the building and shall be operable before the building is first occupied.
Reason. The scheme does not provide full details to alleviate any possible risk associated with the production of landfill gas in accordance with the recommendations of the Environment Agency pursuant to Policy EN7 – Pollution Control of the Bury Unitary Development Plan.

For further information on the application please contact **Helen Goldsbrough** on **0161 253 5277**

Viewpoints



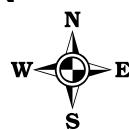
PLANNING APPLICATION LOCATION PLAN

APP. NO 60957

**ADDRESS: Fairfield Primary School
Rochdale Old Road, Bury**

Planning, Environmental and Regulatory Services

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Bury
COUNCIL

60957

Photo 1



Photo 2



Photo 3



Photo 4



Photo 5



ROCHDALE OLD ROAD

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Contractors to check all dimensions on site prior to commencement of works.

All works to be carried out in accordance with current statutory Health and Safety Regulations.

This drawing is to be read in conjunction with all relevant consultants' and / or specialists' drawings / documents and any discrepancies or variations are to be notified to the architect before affected work commences.
- REV.

AMENDMENTS

DATE

18 to 27

d County Primary School

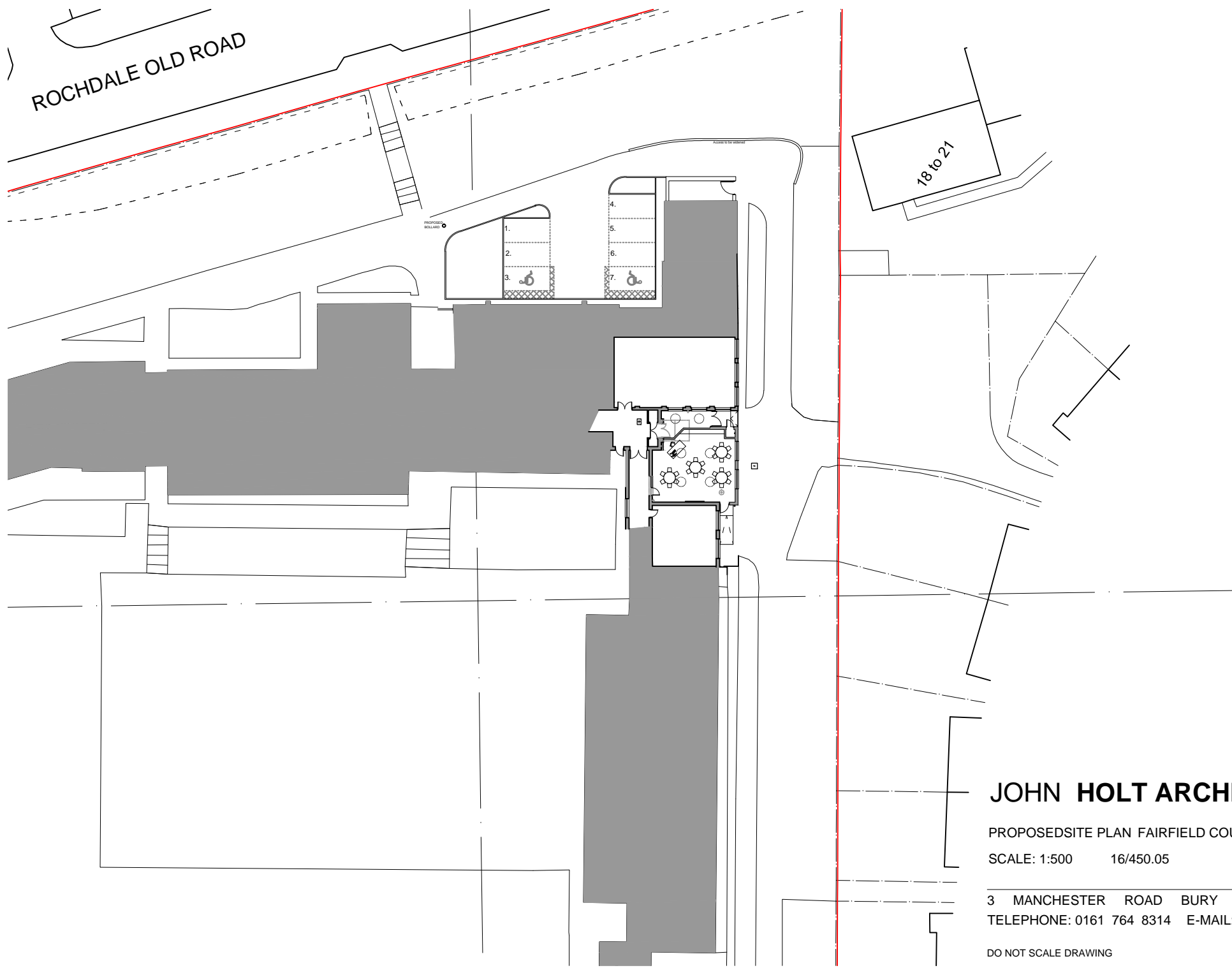
Informal Parking

Existing Parking Layout

Project	PROPOSED ADDITIONAL PARKING: FAIRFIELD C.P PRIMARY SCHOOL
Drawing	EXISTING PARKING LAYOUT
Client	FAIRFIELD C.P. PRIMARY SCHOOL
Dwn. By	LO
Scale	1:200
Drg. No.	16/450.06
Drg. Size	A2
Date	JANUARY 2017

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JOHN HOLT ARCHITECTS

PROPOSED SITE PLAN FAIRFIELD COUNTY PRIMARY

SCALE: 1:500 16/450.05

3 MANCHESTER ROAD BURY LANCASHIRE
TELEPHONE: 0161 764 8314 E-MAIL: mail@holtarchit

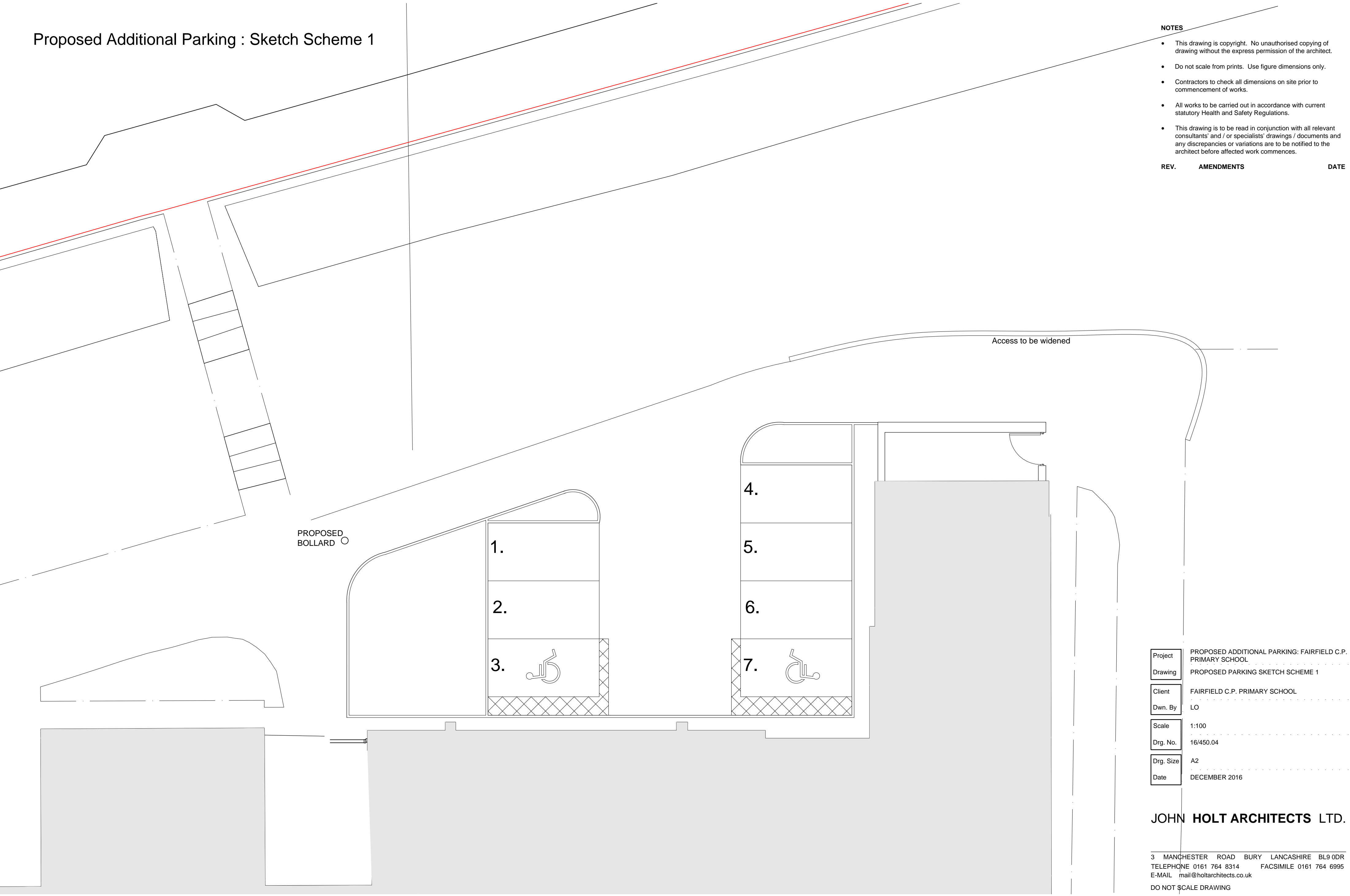
DO NOT SCALE DRAWING

Proposed Additional Parking : Sketch Scheme 1

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- This drawing is to be read in conjunction with all relevant consultants' and / or specialists' drawings / documents and any discrepancies or variations are to be notified to the architect before affected work commences.

REV. AMENDMENTS DATE



Project	PROPOSED ADDITIONAL PARKING: FAIRFIELD C.P. PRIMARY SCHOOL
Drawing	PROPOSED PARKING SKETCH SCHEME 1
Client	FAIRFIELD C.P. PRIMARY SCHOOL
Dwn. By	LO
Scale	1:100
Drg. No.	16/450.04
Drg. Size	A2
Date	DECEMBER 2016

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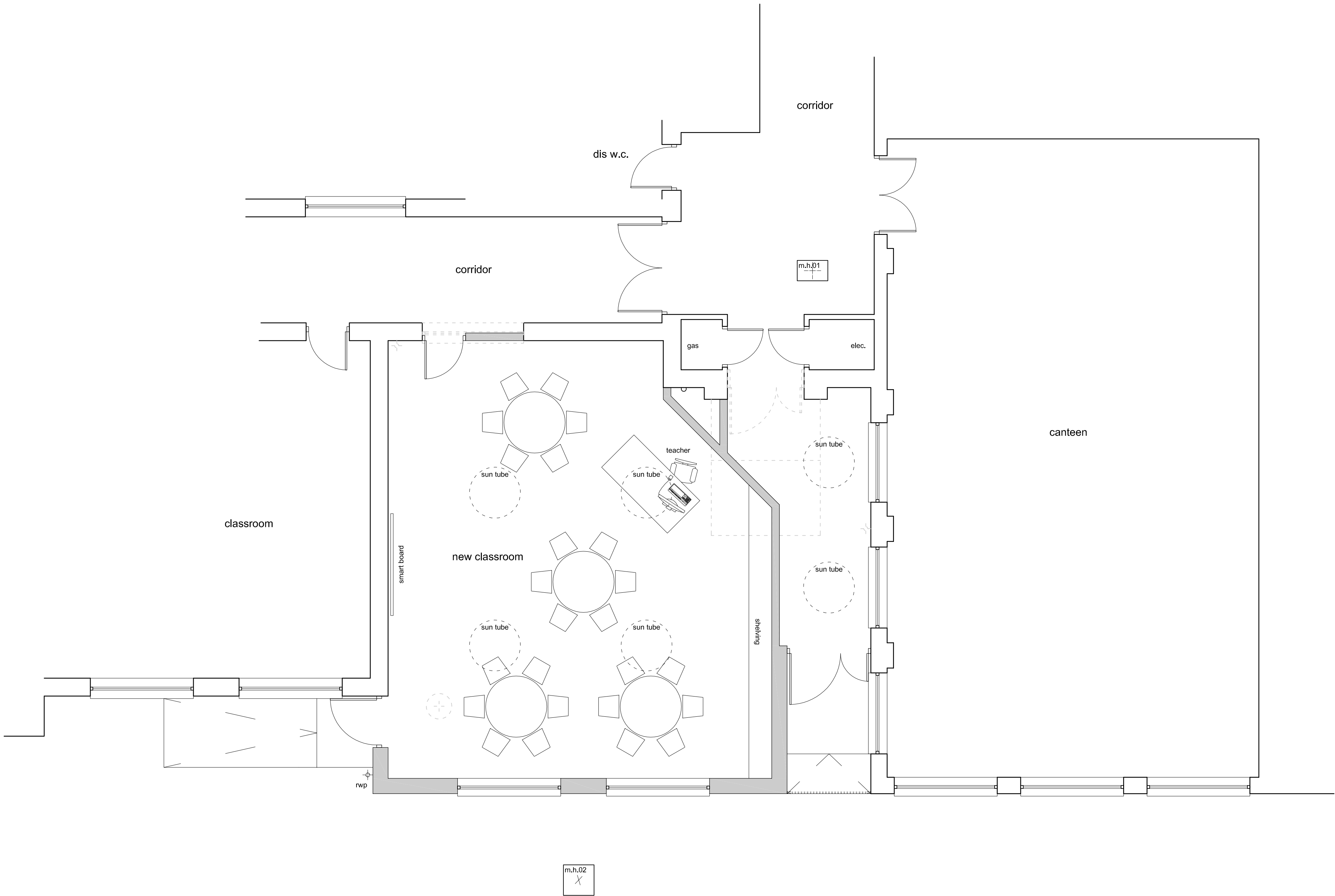
Contractors to check all dimensions on site prior to commencement of works.

All works to be carried out in accordance with current statutory Health and Safety Regulations.

This drawing is to be read in conjunction with all relevant consultants' and / or specialists' drawings / documents and any discrepancies or variations are to be notified to the architect before affected work commences.
- REV.

AMENDMENTS

DATE



PART. PROPOSED FLOOR LAYOUT
1:50



PART. PROPOSED EAST ELEVATION
1:100

Project	PROPOSED CLASSROOM EXTENSION: FAIRFIELD C.P. PRIMARY SCHOOL
Drawing	PART: PROPOSED FLOOR LAYOUT & ELEVATIONS
Client	FAIRFIELD C.P. PRIMARY SCHOOL
Dwn. By	CMc
Scale	1:50 & 1:100
Org. No.	16/450.02
Org. Size	A1
Date	OCTOBER 2016

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